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## DEPARTMENT OF PUBLIC WORKS

Transportation Division  
10455 Armstrong St. Room 200A  
Fairfax, VA 22030

Phone: 703-385-7889

# UNIVERSITY DRIVE TRAFFIC CALMING

## PROJECT DESCRIPTION

THE PURPOSE OF THE UNIVERSITY DRIVE TRAFFIC CALMING IMPROVEMENTS IS TO IMPROVE THE SAFETY OF THE LOCAL RESIDENTS WITH THE POTENTIAL INCREASE IN TRAFFIC ON UNIVERSITY DRIVE.

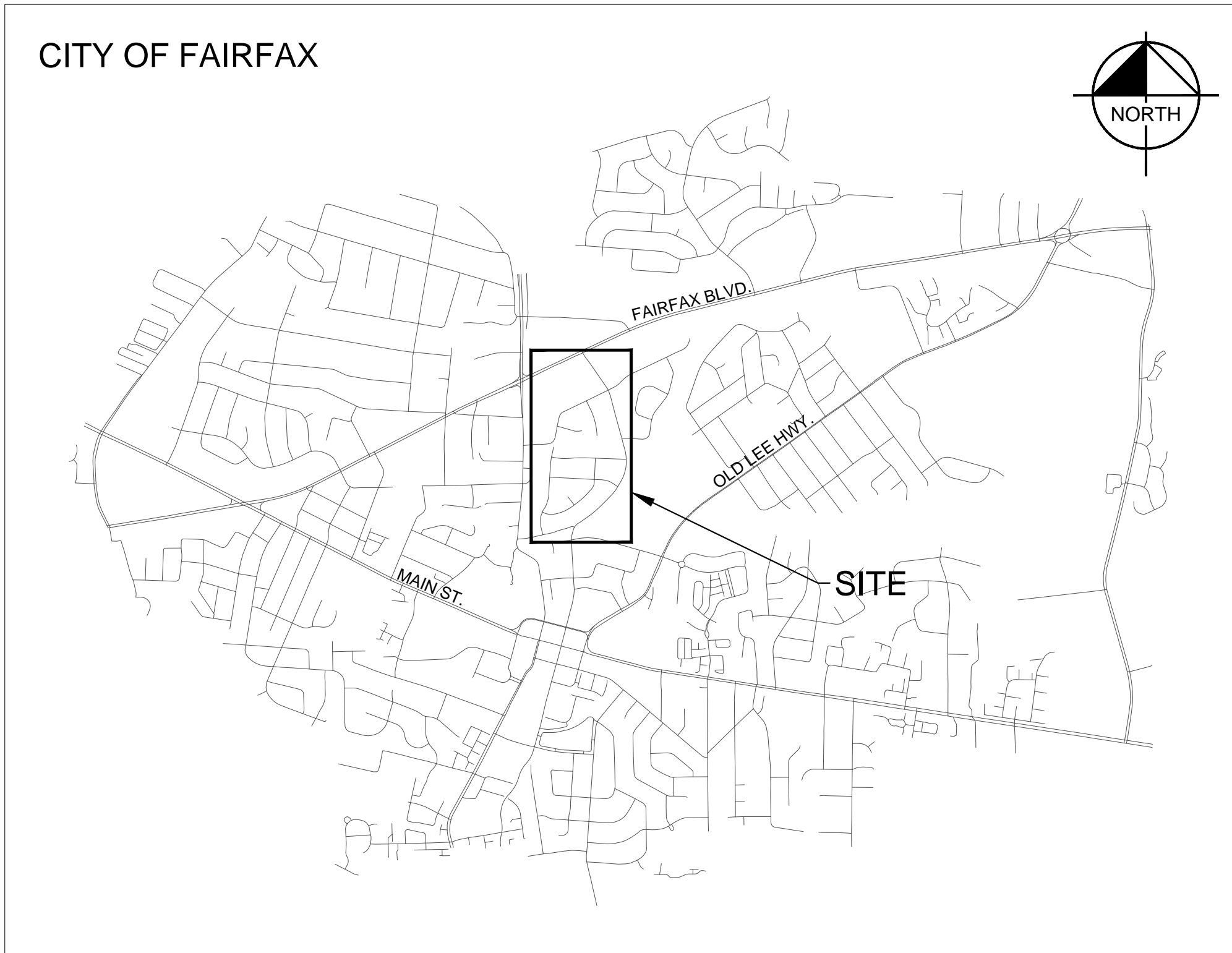
THE WORK OF THIS PROJECT INCLUDES THE FOLLOWING IMPROVEMENTS:

1. SIDEWALK CONSTRUCTION BETWEEN WOOD ROAD AND STRATFORD AVENUE
2. CURB EXTENSION CONSTRUCTION AT THE FORD ROAD INTERSECTION AND STRATFORD AVENUE INTERSECTION
3. CHICANE CONSTRUCTION BETWEEN FORD ROAD AND WOOD ROAD AND JEAN STREET AND STRATFORD AVENUE
4. CURB REALIGNMENT AT THE KENMORE DRIVE INTERSECTION
5. PEDESTRIAN ACCESSIBLE ADA RAMPS
6. SIGNING AND PAVEMENT MARKING
7. LANDSCAPING
8. LIGHTING

THE WORK OF THIS PROJECT WILL INCLUDE, BUT IS NOT LIMITED TO, DEMOLITION, EXCAVATION, RESTORATION, LANDSCAPING, SIGNING AND PAVEMENT MARKING, AND MAINTENANCE OF TRAFFIC.

THE CONTRACTOR SHALL COMPLETE THE WORK IN ACCORDANCE WITH THESE PLANS, REFERENCE SPECIFICATIONS, AND OTHER CONTRACT DOCUMENTS.

## Location Map



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CITY OF FAIRFAX, VIRGINIA  
DEPARTMENT OF PUBLIC WORKS

UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
JUNE 17, 2020

UPC # 113121

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GENERAL NOTES  
(THESE GENERAL NOTES SHALL BE USED WHERE THEY ARE APPLICABLE TO THE PROJECT PLANS)

PERMIT NOTES

1. A STREET OPENING PERMIT IS REQUIRED FOR ANY WORK IN A CITY RIGHT-OF-WAY OR EASEMENT. THE PERMIT CAN BE OBTAINED FROM THE PUBLIC WORKS DEPARTMENT. FOR INFORMATION, CALL 703.385.7980 OR 703.385.7810.
2. ANTICIPATED NOTICE TO PROCEED IN JUNE 2020.
3. ALL SIDEWALKS, CURBS, GUTTERS, DRIVEWAYS, STREETS, RETAINING WALLS, AND STORM INLETS MUST BE INSPECTED BY THE CITY. ALL WORK IN THE CITY STREETS WILL BE PERFORMED MONDAY-FRIDAY BETWEEN THE HOURS OF 9:00AM AND 3:30PM. NO WORK IS TO BE PERFORMED ON WEEKENDS OR HOLIDAYS UNLESS PRE APPROVED BY THE DIRECTOR OF PUBLIC WORKS.
4. INSPECTIONS PERFORMED BY THE FACILITIES INSPECTOR WILL REQUIRE A (4) HOUR NOTICE PRIOR TO INSPECTIONS.

GENERAL STANDARD

1. THE PUBLIC WORKS DIRECTOR MUST BE NOTIFIED (1) WEEK PRIOR TO PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO COMMENCEMENT OF LAND DISTURBING ACTIVITY AND (1) ONE WEEK PRIOR TO FINAL INSPECTION. THE SITE PLAN COORDINATOR IN ZONING MUST BE NOTIFIED (1) ONE WEEK PRIOR TO THE PRE-CONSTRUCTION PHASE.
2. CONTRACTOR TO COORDINATE WITH CITY ON TIMING OF CURB AND GUTTER REPAIR AND REPLACE. APPROXIMATELY 1 MONTH AFTER REPAIRS, PAVEMENT MARKINGS CAN BE INSTALLED.
3. A PRECONSTRUCTION MEETING WILL BE REQUIRED (3) DAYS PRIOR TO ANY CONSTRUCTION. CONTRACTORS WILL NOTIFY THE PUBLIC WORKS DEPARTMENT OR FACILITIES INSPECTOR FOR ALL WORK DONE ON SITE AND OFF SITE (1) DAY PRIOR TO STARTING.
4. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS FOR PARKING CONSTRUCTION EQUIPMENT AND PROVIDE EMPLOYEE PARKING ON SITE.
5. ALL CONSTRUCTION SHALL CONFORM TO THE LATEST CITY OF FAIRFAX STANDARDS, VIRGINIA DEPARTMENT OF TRANSPORTATION AND THE VIRGINIA SEDIMENT AND EROSION CONTROL CURRENT SPECIFICATIONS, EXCEPT AS SHOWN OR ALTERED BY THESE PLANS.

EROSION AND SEDIMENT CONTROL NOTES

1. PRIOR TO ANY LAND DISTURBING OPERATIONS, THE EROSION CONTROLS, AS SPECIFIED BY THE ENGINEERING PLANS, SHALL BE INSTALLED. ALL MECHANICAL AND VEGETATIVE PRACTICES SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS CONTAINED IN THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL AND THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK.
2. ALL AREAS WHICH ARE DISTURBED BY THIS CONSTRUCTION AND WHICH ARE NOT PAVED OR BUILT UPON SHALL BE ADEQUATELY STABILIZED TO CONTROL EROSION AND SEDIMENTATION. ACCEPTABLE STABILIZATION SHALL BE APPROVED BY THE CITY OF FAIRFAX ENGINEER.
3. AS DISTURBED AREAS, NOT CONSTRUCTED ON, ARE FINALLY GRADED, THEY SHALL BE PREPARED, LIME AND FERTILIZER APPLIED, AND SEEDED ACCORDING TO THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL SPECIFICATIONS AND AS APPROVED BY THE CITY OF FAIRFAX ENGINEER.
4. DURING CONSTRUCTION, ALL STORM SEWER INLETS SHALL BE PROTECTED BY SILT TRAPS.
5. THE CONTRACTOR SHALL PROPERLY INSTALL AND MAINTAIN EROSION AND SEDIMENT CONTROLS FOR THE LIFE OF THE PROJECT AND ROUTINELY CHECK CONTROL DEVICES BEFORE, DURING AND AFTER STORM EVENTS.
6. FOR FURTHER REQUIREMENTS AND DETAILS OF TREE PRESERVATION, PLANTING, EROSION AND SEDIMENT CONTROL, SEE THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL AND/OR THE EROSION AND SEDIMENT CONTROL HANDBOOK.

CONSTRUCTION NOTES

1. ALL CONSTRUCTION, INCLUDING PROPOSED LANDSCAPING, SHALL CONFORM TO THE CURRENT EDITION OF THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL AND THE VIRGINIA DEPARTMENT OF TRANSPORTATION STANDARDS AND SPECIFICATIONS.
2. ALL SUBGRADE, SUBBASE, AND BASE MATERIAL SHALL BE PLACED AND COMPACTED TO THE DENSITY SPECIFIED IN THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL.
3. COMPACTION TESTS SHALL BE PERFORMED BY THE CONTRACTOR. SUBGRADE FOR CURB, GUTTER AND SIDEWALK SHALL BE EVERY 50 FEET; SUB-BASE SHALL BE ALTERNATED EVERY 25 FEET. DRIVEWAYS REQUIRE TWO TESTS ON SUBGRADE AND SUB-BASE.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TO CLEAN ALL STREETS AND TO TAKE NECESSARY MEASURES TO ENSURE THE ROADS ARE MAINTAINED IN A CLEAN, MUD AND DUST FREE CONDITION AT ALL TIMES.

5. ALL CURB AND GUTTER SHOWN ON PLANS SHALL BE ON STRAIGHT TANGENT GRADES. THE CONTRACTOR SHALL ROUND ALL VERTICAL BREAKS WITH SMOOTH SPLINE CURVES. PROPOSED TOP OF CURB GRADES SHALL BE ADJUSTED IN THE FIELD TO MAINTAIN A SMOOTH GRADE AND TO MITIGATE THE PONDING OF WATER IN THE ROADWAY.

STORM SEWER

1. A WATERTIGHT CONNECTION SHALL BE MADE AT ALL PIPES ENTERING DRAINAGE STRUCTURES AND BETWEEN SECTIONS OF PIPE.
2. INVERT ELEVATIONS ARE TAKEN AT THE FACE OF THE STRUCTURE.
3. ALL INVERT STRUCTURES SHALL BE SHAPED ACCORDING TO VDOT STANDARD IS-1 AND REQUIREMENTS IN THE CITY OF FAIRFAX PUBLIC FACILITIES MANUAL.
4. MINOR FIELD ADJUSTMENTS IN THE ELEVATION AND ALIGNMENT OF STORM SEWER AND STRUCTURE MIGHT BE NECESSARY TO MEET EXISTING CONDITIONS AND PROPOSED FINAL GRADING. THE CONTRACTOR SHALL NOTIFY CITY OF FAIRFAX PUBLIC WORKS DIRECTOR PRIOR TO MAKING ANY NECESSARY ADJUSTMENTS.
5. TOP OF STRUCTURES SHALL BE SET TO MATCH CURB AND GUTTER, SIDEWALK AND/OR DITCH CONSTRUCTION.

SIGNING AND MARKING:

1. ALL PROPOSED SIGNING AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH CITY OF FAIRFAX CONSTRUCTION STANDARDS.
2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED IN THE PAVEMENT MARKING LEGEND.
3. ANY EXISTING PAVEMENT MARKINGS THAT WILL CONFLICT WITH PROPOSED PAVEMENT MARKINGS SHALL BE COMPLETELY ERADICATED.
4. PROPOSED SIGN LOCATIONS ARE APPROXIMATE AND SHALL BE MODIFIED IN THE FIELD TO AVOID CONFLICT WITH UNDERGROUND UTILITIES OR OTHER CONSTRUCTIONS.
5. PROPOSED SIGNS SHALL BE INSTALLED SO THAT NO PORTION OF THE SIGN PANEL OVERHANGS ADJACENT ROADWAY PAVEMENT, I.E SHALL NOT HANG IN FRONT OF A FACE OF CURB.
6. PROPOSED SIGNS AND POSTS SHALL BE INSTALLED SO THEY DO NOT BLOCK THE VISIBILITY OF ANY EXISTING SIGNS OR SIGNALS.
7. PROPOSED SIGNS AND POSTS SHALL BE CLEAR OF EXISTING FIRE HYDRANTS, SURFACE UTILITY, AND OVERHEAD UTILITY EQUIPMENT.
8. FOR NEW POST INSTALLATION, THE CONTRACTOR SHALL VERIFY THAT THERE ARE NOT CONFLICTING UNDERGROUND OR OVERHEAD UTILITIES.
9. ALL SIGN LOCATIONS SHOWN ON THE PLANS ARE SCHEMATIC AND MAY NOT REFLECT ACTUAL FIELD LOCATION. THE CONTRACTOR SHALL VERIFY EACH LOCATION PRIOR TO INSTALLING A SIGN. IF AT ANY POINT THE CONTRACTOR FINDS A CONFLICT, THE CONTRACT SHALL CONTACT THE ENGINEER PRIOR TO INSTALLING THE PROPOSED SIGN. PROPOSED SIGN LOCATIONS CAN BE ADJUSTED AS APPROVED BY THE ENGINEER.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING WITH MATCHING MATERIALS ANY PAVEMENT, PAVEMENT MARKINGS, CURB AND GUTTER, SIDEWALK, ETC. THAT ARE DAMAGED DURING CONSTRUCTION.

MAINTENANCE OF TRAFFIC

1. ALL TRAFFIC MAINTENANCE SHALL CONFORM WITH THE TYPICAL TRAFFIC CONTROLS SPECIFIED IN THE PLANS AS WELL AS THE FOLLOWING AND LATEST REVISIONS TO: THE VIRGINIA WORK AREA PROTECTION MANUAL, VA ROAD AND BRIDGE SPECIFICATIONS, VA ROAD AND BRIDGE STANDARDS AND THE FHWA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
2. ALL REQUIRED CONSTRUCTION SIGNING, TEMPORARY PAVEMENT WIDENING, TEMPORARY LAND STABILIZATION, TRAFFIC BARRIERS, TEMPORARY PAVEMENT MARKINGS, ERADICATION, ETC., SHALL BE INCLUDED IN THE CONTRACTOR'S BID PRICE FOR TRAFFIC MAINTENANCE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DAILY TRAFFIC CONTROL TO PROPERLY MAINTAIN TRAFFIC THROUGHOUT THE PROJECT.

(CONTINUED NEXT PAGE)



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CITY OF FAIRFAX, VIRGINIA  
DEPARTMENT OF PUBLIC WORKS

UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
GENERAL NOTES

UPC # 113121

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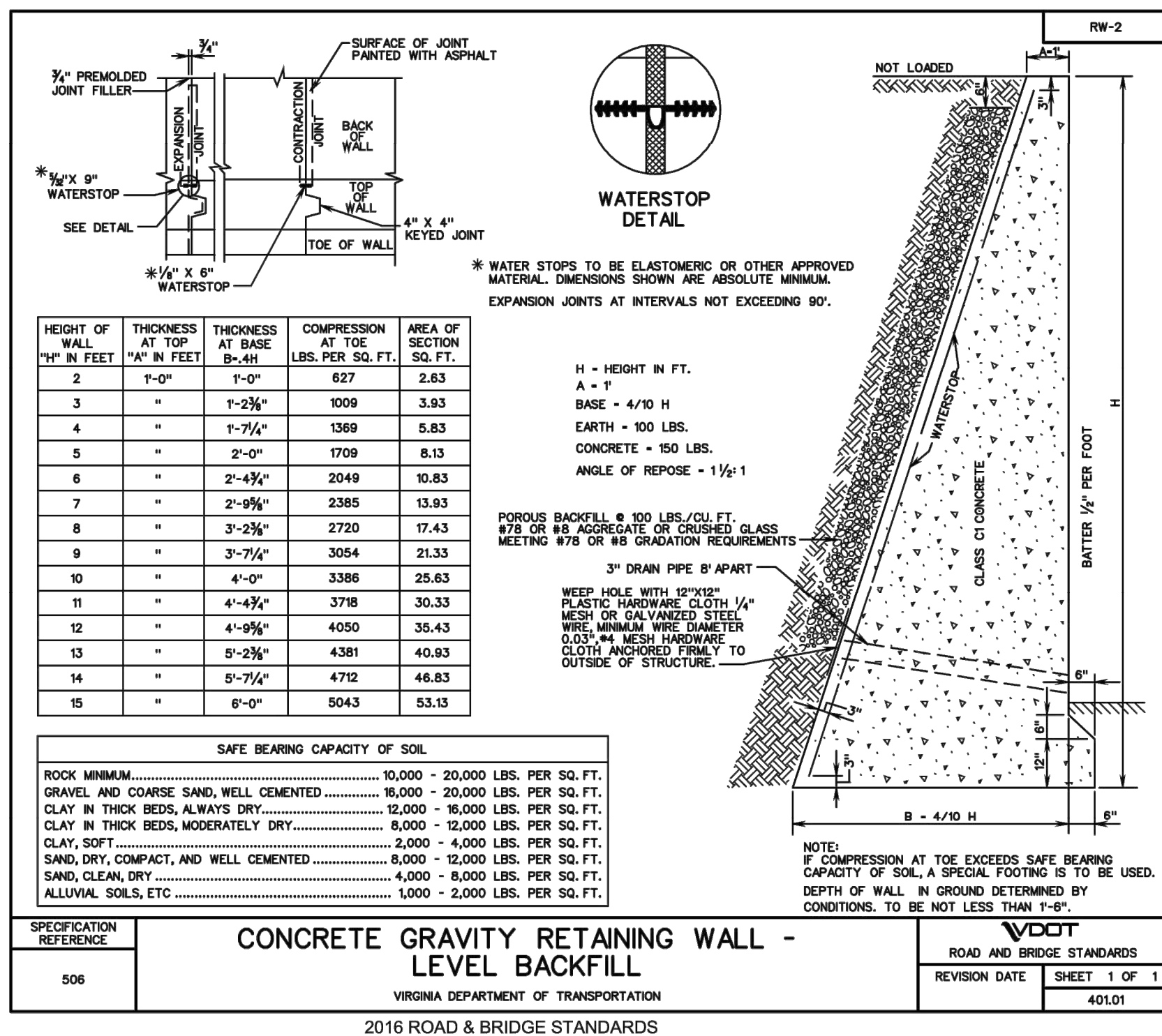
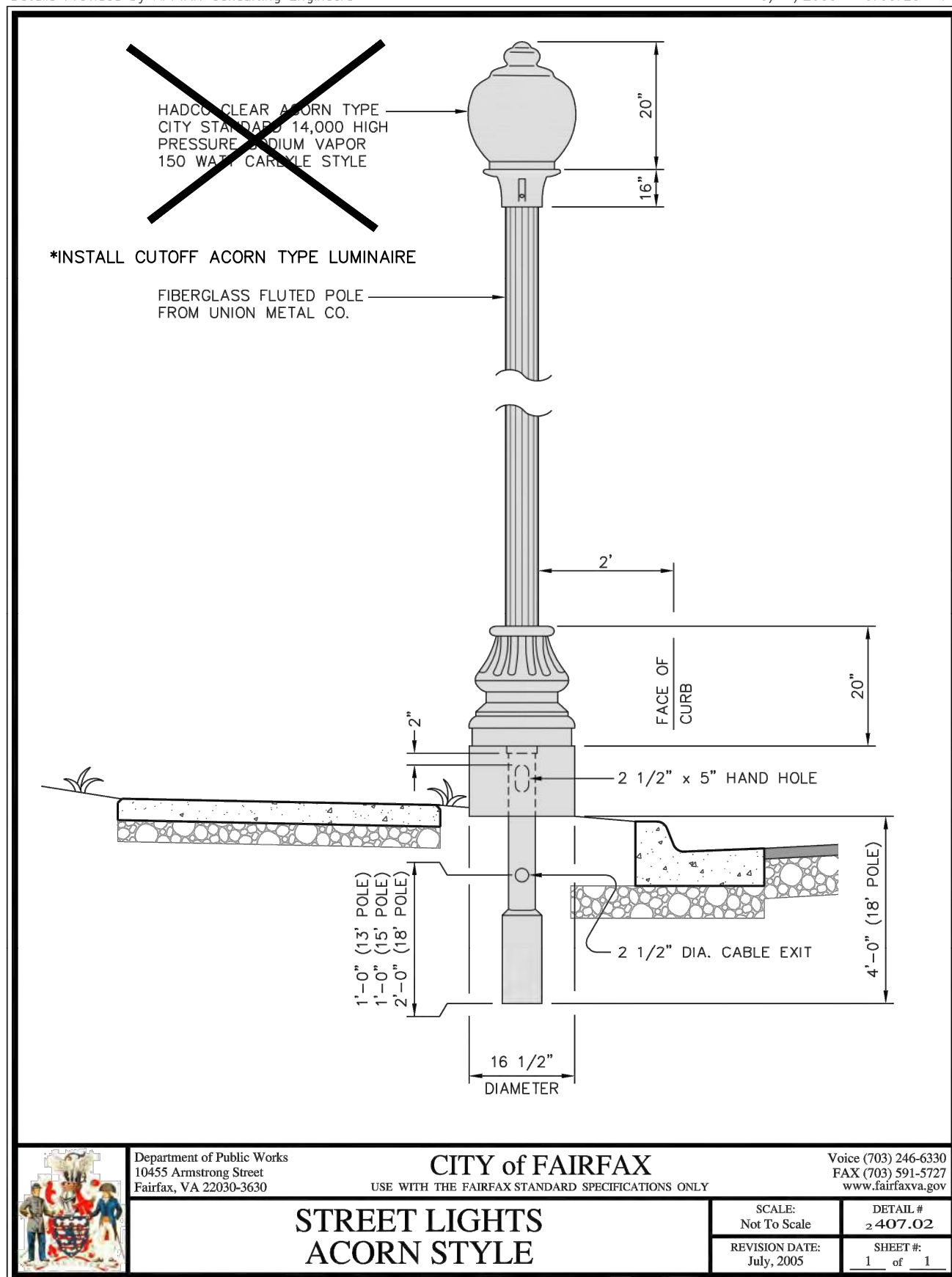
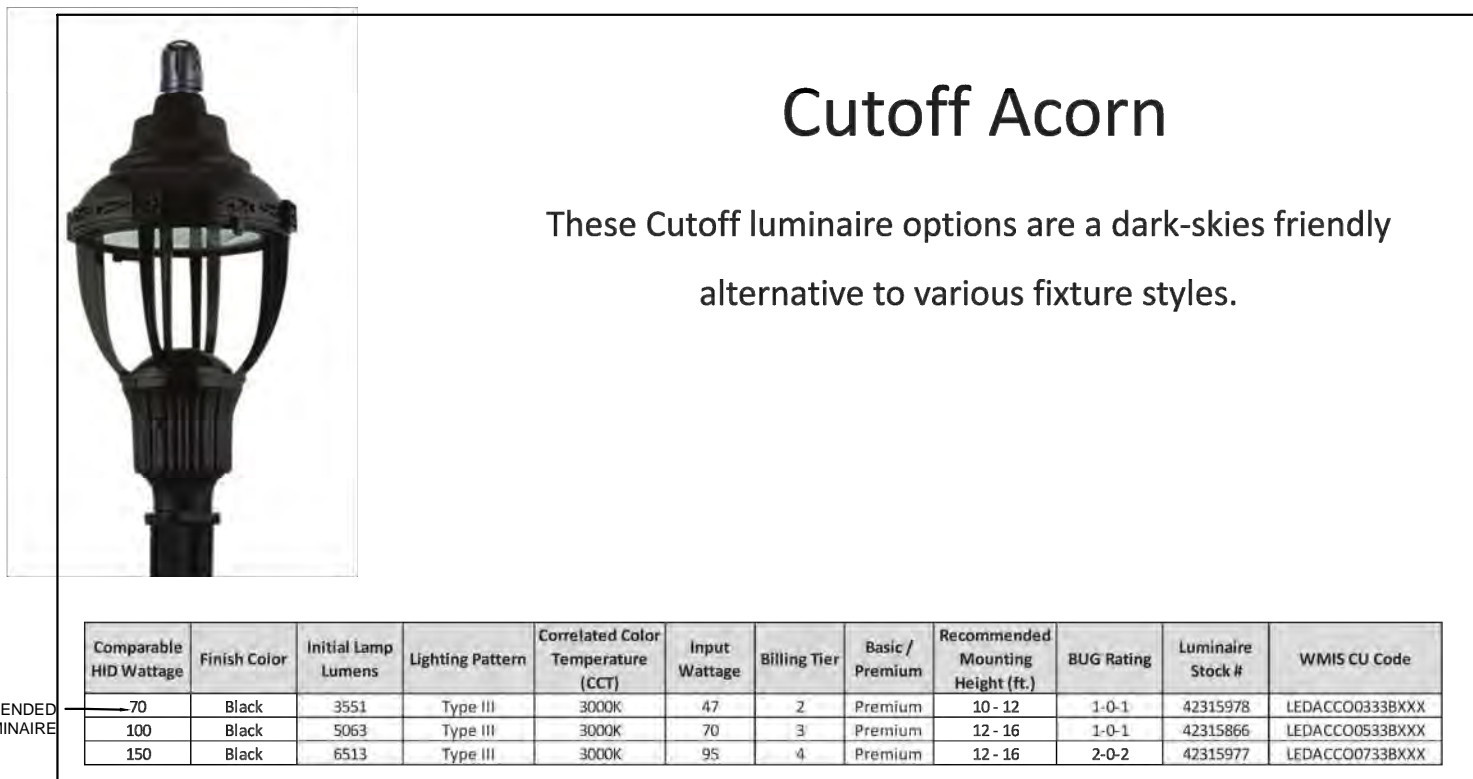
## \*MODIFIED STANDARD





## GENERAL NOTES AND STANDARD DETAILS (CONTINUED)

## SEQUENCE OF CONSTRUCTION



1. IDENTIFY THE WORK ZONE LOCATION, LENGTHS, AND WIDTHS
2. NOTE THE HOURS THE CONSTRUCTION AREA WILL BE ACTIVE:  
CONSTRUCTION AREA SHALL BE CONSIDERED ACTIVE WHEN ANY IMPACT TO TRAFFIC OCCURS  
CONSTRUCTION AREA HOURS HAVE THE FOLLOWING LIMITATIONS:

|            | LANE CLOSURES      |              |              |              |
|------------|--------------------|--------------|--------------|--------------|
|            | MONDAY TO THURSDAY | FRIDAY       | SATURDAY     | SUNDAY       |
| DAY TIME   |                    |              | *NOT ALLOWED | *NOT ALLOWED |
| NIGHT TIME | *NOT ALLOWED       | *NOT ALLOWED | *NOT ALLOWED | *NOT ALLOWED |

\*NIGHT TIME AND WEEKEND WORK SHALL NOT BE ALLOWED UNLESS APPROVED BY THE CITY OF FAIRFAX

NO LANE CLOSURES WILL BE ALLOWED FROM NOON ON THE DAY BEFORE A HOLIDAY UNTIL NOON ON THE WORKDAY FOLLOWING THE HOLIDAY. HOLIDAYS INCLUDE ALL STATE AND FEDERAL HOLIDAYS.

DESIGNATION OF PEAK HOUR TIMES:  
PEAK HOURS ARE 6:00AM THROUGH 9:00AM AND 3:30PM THROUGH 6:30PM.

3. THIS TMP/SEQUENCE OF CONSTRUCTION PLAN IS INTENDED AS A GUIDE. IT IS NOT TO ENUMERATE EVERY DETAIL WHICH MUST BE CONSIDERED IN THE CONSTRUCTION OF EACH PHASE, BUT ONLY SHOW THE GENERAL HANDLING OF EXISTING TRAFFIC. IF THE CONTRACTOR IS TO DEVIATE FROM THE APPROVED TMP/SOC, A NEW REVISED TMP/SOC MUST BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL. CONSTRUCTION SEQUENCE TO BE USED BY THE CONTRACTOR SHALL BE APPROVED BY THE CITY PRIOR TO PROCEEDING WITH WORK.
- PHASE 1
1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT THE PROPOSED CURB REALIGNMENT AT THE INTERSECTION OF UNIVERSITY DRIVE AND KENMORE DRIVE.
  2. CONTRACTOR IS TO IMPLEMENT VWAMP TTC-1.0 TO INSTALL THE PROPOSED LIGHT POLE AT THE INTERSECTION OF UNIVERSITY DRIVE AND KENMORE DRIVE.
  3. CONTRACTOR TO ERADICATE CONFLICTING PAVEMENT MARKINGS AND INSTALL TEMPORARY PAVEMENT MARKINGS.

## PHASE 2

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-23.1 TO CONSTRUCT THE PROPOSED CURB AND GUTTER, SIDEWALK, AND CURB RAMP LOCATED ON THE EAST SIDE OF UNIVERSITY DRIVE AT THE INTERSECTION OF UNIVERSITY DRIVE AND FORD ROAD.
2. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT THE PROPOSED CURB AND GUTTER, INLET EXTENSION, AND SIDEWALK LOCATED AT THE SOUTH WEST CORNER OF THE UNIVERSITY DRIVE AND FORD ROAD INTERSECTION.
3. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT THE PROPOSED CURB AND GUTTER, INLET EXTENSION, AND SIDEWALK LOCATED AT THE NORTH WEST CORNER OF THE UNIVERSITY DRIVE AND FORD ROAD INTERSECTION.
4. CONTRACTOR TO ERADICATE CONFLICTING PAVEMENT MARKINGS AND INSTALL TEMPORARY PAVEMENT MARKINGS.
5. CONTRACTOR TO RELOCATE EXISTING SIGNAGE.

### PHASE 3

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-1.0 TO INSTALL THE PROPOSED LIGHT POLES BETWEEN THE INTERSECTIONS OF UNIVERSITY DRIVE AND FORD ROAD AND UNIVERSITY DRIVE AND JEAN STREET.
2. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-23.1 TO CONSTRUCT THE PROPOSED CURB AND RAIN GARDENS BETWEEN THE INTERSECTIONS OF UNIVERSITY DRIVE AND FORD ROAD AND UNIVERSITY DRIVE AND JEAN STREET.
3. CONTRACTOR TO ERADICATE CONFLICTING PAVEMENT MARKINGS AND INSTALL TEMPORARY PAVEMENT MARKINGS.
4. CONTRACTOR TO INSTALL PROPOSED SIGNAGE.

## PHASE 4

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT PROPOSED CURB RAMP AT THE INTERSECTION OF UNIVERSITY DRIVE AND JEAN STREET.
2. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-23.1 TO CONSTRUCT PROPOSED SIDEWALK, CURB RAMP, AND RETAINING WALL AT THE INTERSECTION OF

UNIVERSITY DRIVE AND JEAN STREET.  
3. CONTRACTOR TO INSTALL TEMPORARY PAVEMENT MARKINGS.

## PHASE 5

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-1.0 AND TTC-23.1 TO CONSTRUCT THE PROPOSED SIDEWALK, RETAINING WALLS, DRIVEWAY APRONS AND LIGHT POLES.

## PHASE 6

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT THE PROPOSED CURB AND GUTTER, SIDEWALK, AND CURB RAMPS AT THE SOUTH WEST CORNER OF THE STRATFORD AVENUE INTERSECTION.
2. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-28.0 TO CONSTRUCT THE PROPOSED CURB AND GUTTER, SIDEWALK, AND CURB RAMPS AT THE THREE REMAINING CORNERS OF THE STRATFORD AVENUE INTERSECTION INDIVIDUALLY.
3. CONTRACTOR TO ERADICATE CONFLICTING PAVEMENT MARKINGS AND INSTALL TEMPORARY PAVEMENT MARKINGS.
4. CONTRACTOR TO RELOCATE EXISTING SIGNAGE.

## PHASE 7

1. CONTRACTOR IS TO IMPLEMENT VWAPM TTC-23.1 TO CONSTRUCT THE PROPOSED CURB AND RAIN GARDENS BETWEEN THE INTERSECTIONS OF UNIVERSITY DRIVE AND JEAN STREET AND UNIVERSITY DRIVE AND STRATFORD AVENUE.
2. CONTRACTOR TO ERADICATE CONFLICTING PAVEMENT MARKINGS AND INSTALL TEMPORARY PAVEMENT MARKINGS.
3. CONTRACTOR TO INSTALL PROPOSED SIGNAGE.

PHASE 8 (BY CITY, NOT IN CONTRACT)

1. REPAVE ROAD, INSTALL FINAL PAVEMENT MARKINGS 4A-4F.

NOTES:

1. WORK WITHIN THE CITY OF FAIRFAX RIGHT OF WAY SHALL COMPLY WITH ALL RIGHT OF WAY PERMIT REQUIREMENTS AND GUIDELINES.
2. CONTRACTOR SHALL REMOVE OR ADJUST (AS NEEDED) ALL TEMPORARY TRAFFIC CONTROLS UPON COMPLETION OF EACH PHASE.
3. CONTRACTOR SHALL DESIGNATE A PERSON ASSIGNED TO THE PROJECT WHO WILL HAVE THE PRIMARY RESPONSIBILITY FOR IMPLEMENTING THE TMP/SOC. THIS PERSON SHALL COORDINATE WITH THE CITY OF FAIRFAX CONSTRUCTION INSPECTOR FOR THE DURATION OF THE CONSTRUCTION.
4. CONTRACTOR SHALL ENSURE THAT ALL PERSONNEL ASSIGNED TO THE PROJECT ARE TRAINED IN TRAFFIC CONTROL TO A LEVEL COMMENSURATE WITH THEIR RESPONSIBILITIES IN ACCORDANCE WITH VDOT'S WORK ZONE TRAFFIC CONTROL TRAINING GUIDELINES.
5. CONTRACTOR SHALL SCHEDULE ALL PHASES OF CONSTRUCTION IN SUCH A MANNER THAT ANY UNDERGROUND AND OVERHANGING UTILITIES WILL NOT BE INTERRUPTED.



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CITY OF FAIRFAX, VIRGINIA  
DEPARTMENT OF PUBLIC WORKS

# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

### GENERAL NOTES

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RIGHT OF WAY DATA

| PARCEL NO. | PARCEL ID   | LANDOWNER                                   | SHEET  | EASEMENT AREA |             |
|------------|-------------|---|--------|---------------|-------------|
|            |             |   |        | PERMANENT     | TEMPORARY   |
|            |             |   |        | SQUARE FEET   | SQUARE FEET |
| 1          | 57-2-09-001 | TRUSTEES OF THE FAIRFAX METHODIST CHURCH    | 3F     | 528           | 149         |
| 2          | 57-2-09-002 | JOHN L. FIGEL                               | 3F     | 300           | 375         |
| 3          | 57-2-09-003 | LLOYD TODD WRIGHT AND JENNIFER LYNNE WRIGHT | 3F     |               | 472         |
| 4          | 57-2-09-004 | VINCENT MAI                                 | 3E, 3F |               | 570         |
| 5          | 57-2-09-005 | MAIRA E. RUBIO SANCHEZ & NELSON J. JACOME   | 3E     |               | 589         |
| 6          | 57-2-09-006 | TIFFANY CHANG                               | 3E     |               | 517         |
| 7          | 57-2-09-007 | ROBERT A. KEMP                              | 3E     |               | 156         |
| 8          | 57-2-09-008 | NEUSA MEDEIROS                              | 3E     |               | 639         |
| 9          | 57-2-09-009 | MICHELLE MINH TU PHAM & AMY CHAUHANG TRINH  | 3D, 3E | 381           | 426         |
|            |             | TOTAL                                       |        | 1209          | 3893        |



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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
RIGHT OF WAY DATA

UPC # 113121

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GRAPHIC SCALE IN FEET

0 10 20

A horizontal line with vertical tick marks at 0, 10, and 20 feet. The segment between 0 and 10 is divided into two equal parts by a tick mark at 5 feet. The segment between 10 and 20 is divided into two equal parts by a tick mark at 15 feet. The segments from 0 to 5, 5 to 10, 10 to 15, and 15 to 20 are all of equal length.



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UNIVERSITY DRIVE ALIGNMENT


| LINE TABLE |        |                |               |             |
|------------|--------|----------------|---------------|-------------|
| LINE       | LENGTH | BEARING        | START STATION | END STATION |
| L1         | 130.54 | N16°34'56.97"E | 0+00.00       | 1+30.54     |
| L2         | 222.29 | N44°48'39.03"E | 7+49.21       | 9+71.51     |
| L3         | 114.93 | N32°51'27.18"E | 11+09.41      | 12+24.33    |
| L4         | 130.70 | N34°16'47.95"W | 29+09.27      | 30+39.97    |
| L5         | 944.21 | N15°07'29.19"W | 18+31.34      | 27+75.54    |

| CURVE TABLE |          |         |               |         |           |         |
|-------------|----------|---------|---------------|---------|-----------|---------|
| CURVE       | RADIUS   | LENGTH  | CHORD BEARING | CHORD   | DELTA     | TANGENT |
| C1          | 138.00'  | 102.14' | N37°47'09"E   | 99.82'  | 42°24'23" | 53.54'  |
| C2          | 2087.40' | 516.53' | N51°54'00"E   | 515.22' | 14°10'41" | 259.59' |
| C3          | 661.00'  | 137.90' | N38°50'03"E   | 137.65' | 11°57'12" | 69.20'  |
| C4          | 691.00'  | 421.21' | N15°23'41"E   | 414.72' | 34°55'32" | 217.38' |
| C5          | 815.30'  | 185.79' | N8°35'47"W    | 185.39' | 13°03'24" | 93.30'  |
| C6          | 400.00'  | 133.73' | N24°42'09"W   | 133.11' | 19°09'19" | 67.49'  |

BACK OF CURB ALIGNMENT

| LINE TABLE |        |                |               |             |
|------------|--------|----------------|---------------|-------------|
| LINE       | LENGTH | BEARING        | START STATION | END STATION |
| L6         | 77.02  | N14°57'03.64"W | 200+00.00     | 200+77.02   |
| L7         | 15.95  | N15°01'43.49"W | 200+77.02     | 200+92.97   |
| L8         | 73.59  | N15°09'54.45"W | 200+92.97     | 201+66.56   |
| L9         | 19.49  | N14°54'11.41"W | 201+66.56     | 201+86.05   |
| L10        | 36.89  | N15°12'50.37"W | 201+86.05     | 202+22.94   |
| L11        | 16.16  | N15°15'18.09"W | 202+22.94     | 202+39.09   |
| L12        | 52.70  | N15°10'23.62"W | 202+39.09     | 202+91.79   |
| L13        | 14.15  | N15°05'16.52"W | 202+91.79     | 203+05.94   |
| L14        | 25.66  | N15°03'20.49"W | 203+05.94     | 203+31.60   |
| L15        | 26.89  | N15°22'52.29"W | 203+31.60     | 203+58.49   |
| L16        | 72.15  | N15°10'51.62"W | 203+58.49     | 204+30.64   |
| L17        | 15.94  | N14°42'48.32"W | 204+30.64     | 204+46.58   |
| L18        | 42.48  | N15°18'16.24"W | 204+46.58     | 204+89.06   |
| L19        | 101.29 | N15°02'28.96"W | 204+89.06     | 205+90.35   |
| L20        | 15.97  | N15°11'55.28"W | 205+90.35     | 206+06.32   |
| L21        | 75.53  | N15°10'59.65"W | 206+06.32     | 206+81.85   |
| L22        | 16.09  | N14°55'31.58"W | 206+81.85     | 206+97.94   |
| L23        | 38.90  | N14°59'27.42"W | 206+97.94     | 207+36.84   |
| L24        | 17.62  | N15°01'26.44"W | 207+36.84     | 207+54.46   |
| L25        | 143.17 | N15°11'14.07"W | 207+54.46     | 208+97.63   |
| L26        | 41.16  | N15°45'32.75"W | 208+97.63     | 209+38.79   |
| L27        | 19.68  | S65°00'15.79"W | 210+08.05     | 210+27.73   |

| CURVE TABLE |        |        |               |        |           |         |
|-------------|--------|--------|---------------|--------|-----------|---------|
| CURVE       | RADIUS | LENGTH | CHORD BEARING | CHORD  | DELTA     | TANGENT |
| C7          | 33.19' | 24.23' | N40°23'38"W   | 23.70' | 41°50'04" | 12.69'  |
| C8          | 28.53' | 9.69'  | N70°13'22"W   | 9.65'  | 19°27'50" | 4.89'   |
| C9          | 59.67' | 35.33' | S81°58'07"W   | 34.82' | 33°55'42" | 18.20'  |



CITY OF FAIRFAX

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION ALIGNMENT DATA

UPC # 113121

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NOTE: THE TOTAL TREATMENT AREA (0.63 AC) FOR THE PROPOSED BMP'S IS GREATER THAN THE SITE DISTURBED AREA OF 0.45 AC DUE TO OFF-SITE IMPERVIOUS FLOW WHICH IS CURRENTLY UNTREATED NOR DETAINED IN EXISTING SWM/BMP FACILITIES

DEQ Virginia Runoff Reduction Method Re-Development Compliance Spreadsheet - Version 3.0

BMP Design Specifications List: 2013 Draft Stds & Specs

Site Summary - Linear Development Project\*\*\*

Project Title: University Drive Traffic Calming  
Date: 43859

|                         |      |
|-------------------------|------|
| Total Rainfall (in)     | 43   |
| Total Disturbed Acreage | 0.63 |

Site Land Cover Summary

| Pre-ReDevelopment Land Cover (acres) | A soils | B Soils | C Soils | D Soils | Totals | % of Total |
|--------------------------------------|---------|---------|---------|---------|--------|------------|
| Forest/Open (acres)                  | 0.00    | 0.00    | 0.00    | 0.00    | 0.00   | 0          |
| Managed Turf (acres)                 | 0.00    | 0.00    | 0.00    | 0.28    | 0.28   | 44         |
| Impervious Cover (acres)             | 0.00    | 0.00    | 0.00    | 0.35    | 0.35   | 56         |
|                                      |         |         |         |         | 0.63   | 100        |

| Post-ReDevelopment Land Cover (acres) | A soils | B Soils | C Soils | D Soils | Totals | % of Total |
|---------------------------------------|---------|---------|---------|---------|--------|------------|
| Forest/Open (acres)                   | 0.00    | 0.00    | 0.00    | 0.00    | 0.00   | 0          |
| Managed Turf (acres)                  | 0.00    | 0.00    | 0.00    | 0.20    | 0.20   | 32         |
| Impervious Cover (acres)              | 0.00    | 0.00    | 0.00    | 0.43    | 0.43   | 68         |
|                                       |         |         |         |         | 0.63   | 100        |

Site Tv and Land Cover Nutrient Loads

|                                     | Final Post-Development<br>(Post-ReDevelopment<br>& New Impervious) | Post-<br>ReDevelopment | Post-<br>Development<br>(New Impervious) | Adjusted Pre-<br>ReDevelopment |
|-------------------------------------|--|------------------------|--|--------------------------------|
| Site Rv                             | 0.73   | 0.70                   | 0.95                                     | 0.70                           |
| Treatment Volume (ft <sup>3</sup> ) | 1,664  | 1,388                  | 276                                      | 1,388                          |
| TP Load (lb/yr)                     | 1.05   | 0.87                   | 0.17                                     | 0.87                           |

| Pre-<br>ReDevelopment<br>TP Load per acre<br>(lb/acre/yr) | Final Post-Development<br>TP Load per acre<br>(lb/acre/yr) | Post-ReDevelopment TP<br>Load per acre<br>(lb/acre/yr) |
|---|--|--|
| 1.59  | 1.66   | 1.59   |

|  |      |        |        |
|--|------|--------|--------|
| Total TP Load Reduction Required (lb/yr) | 0.31 | N/A*** | N/A*** |
|--|------|--------|--------|

\*\*\*This is a linear development project

|                 | Final Post-Development Load<br>(Post-ReDevelopment & New Impervious) | Pre-<br>ReDevelopment |
|-----------------|--|-----------------------|
| TN Load (lb/yr) | 7.48   | 6.57                  |

Site Compliance Summary - \*\*\*Linear Development Project

|  |     |
|--|-----|
| Maximum % Reduction Required Below<br>Pre-ReDevelopment Load | 20% |
|--|-----|

|  |      |
|--|------|
| Total Runoff Volume Reduction (ft <sup>3</sup> ) | 593  |
| Total TP Load Reduction Achieved (lb/yr)         | 0.51 |
| Total TN Load Reduction Achieved (lb/yr)         | 4.26 |
| Remaining Post Development TP Load<br>(lb/yr)    | 0.53 |
| Remaining TP Load Reduction (lb/yr)<br>Required  | 0.00 |

\*\* TARGET TP REDUCTION EXCEEDED BY 0.2 LB/YEAR \*\*

Drainage Area Summary

|                          | D.A. A | D.A. B | D.A. C | D.A. D | D.A. E | Total |
|--------------------------|--------|--------|--------|--------|--------|-------|
| Forest/Open (acres)      | 0.00   | 0.00   | 0.00   | 0.00   | 0.00   | 0.00  |
| Managed Turf (acres)     | 0.20   | 0.00   | 0.00   | 0.00   | 0.00   | 0.20  |
| Impervious Cover (acres) | 0.43   | 0.00   | 0.00   | 0.00   | 0.00   | 0.43  |
| Total Area (acres)       | 0.63   | 0.00   | 0.00   | 0.00   | 0.00   | 0.63  |

Drainage Area Compliance Summary

|                         | D.A. A | D.A. B | D.A. C | D.A. D | D.A. E | Total |
|-------------------------|--------|--------|--------|--------|--------|-------|
| TP Load Reduced (lb/yr) | 0.51   | 0.00   | 0.00   | 0.00   | 0.00   | 0.51  |
| TN Load Reduced (lb/yr) | 4.26   | 0.00   | 0.00   | 0.00   | 0.00   | 4.26  |

Drainage Area A Summary

Land Cover Summary

|                          | A Soils | B Soils | C Soils | D Soils | Total | % of Total |
|--------------------------|---------|---------|---------|---------|-------|------------|
| Forest/Open (acres)      | 0.00    | 0.00    | 0.00    | 0.00    | 0.00  | 0          |
| Managed Turf (acres)     | 0.00    | 0.00    | 0.00    | 0.20    | 0.20  | 32         |
| Impervious Cover (acres) | 0.00    | 0.00    | 0.00    | 0.43    | 0.43  | 68         |
|                          |         |         |         |         | 0.63  |            |

BMP Selections

| Practice   | Managed Turf<br>Credit Area<br>(acres) | Impervious Cover<br>Credit Area<br>(acres) | BMP Treatment<br>Volume (ft <sup>3</sup> ) | TP Load from<br>Upstream<br>Practices (lbs) | Untreated TP Load<br>to Practice (lbs) | TP Removed<br>(lb/yr) | TP Remaining<br>(lb/yr) | Downstream Treatment<br>to be Employed |
|--|--|--|--|---|--|-----------------------|-------------------------|--|
| 2i. To Stormwater Planter,<br>Urban Bioretention (Spec #9, Appendix A) |  | 0.43                                       | 1,482.86                                   | 0.00  | 0.93                                   | 0.51                  | 0.42                    |  |

|   |      |
|---|------|
| Total Impervious Cover Treated (acres)              | 0.43 |
| Total Turf Area Treated (acres)                     | 0.00 |
| Total TP Load Reduction Achieved in D.A.<br>(lb/yr) | 0.51 |
| Total TN Load Reduction Achieved in D.A.<br>(lb/yr) | 4.26 |

Runoff Volume and CN Calculations

|                            | 1-year storm | 2-year storm | 10-year storm |
|----------------------------|--------------|--------------|---------------|
| Target Rainfall Event (in) | 2.50         | 3.14         | 4.82          |

| Drainage Areas        | RV & CN           | Drainage Area A | Drainage Area B | Drainage Area C | Drainage Area D | Drainage Area E |
|-----------------------|-------------------|-----------------|-----------------|-----------------|-----------------|-----------------|
| CN                    |                   | 92              | 0               | 0               | 0               | 0               |
| RR (ft <sup>3</sup> ) |                   | 593             | 0               | 0               | 0               | 0               |
| 1-year return period  | RV w/o RR (ws-in) | 1.79            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | RV w RR (ws-in)   | 1.55            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | CN adjusted       | 89              | 0               | 0               | 0               | 0               |
| 2-year return period  | RV w/o RR (ws-in) | 2.29            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | RV w RR (ws-in)   | 2.03            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | CN adjusted       | 89              | 0               | 0               | 0               | 0               |
| 10-year return period | RV w/o RR (ws-in) | 3.91            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | RV w RR (ws-in)   | 3.65            | 0.00            | 0.00            | 0.00            | 0.00            |
|                       | CN adjusted       | 89              | 0               | 0               | 0               | 0               |

COMPLIANCE WITH THE WATER QUALITY DESIGN CRITERIA IS DETERMINED UTILIZING THE VIRGINIA RUNOFF REDUCTION METHOD. MICRO-BIORETENTION IS THE SELECTED BMP TO REDUCE POLLUTANT LOADS AND/OR RUNOFF VOLUME. THE PEAK DISCHARGE REQUIREMENTS ARE DETERMINED IN ACCORDANCE WITH THE VDOT DRAINAGE MANUAL.

THE MAXIMUM PEAK FLOW RATE IS DETERMINED FROM THE ENERGY BALANCE EQUATION IN THE VDOT DRAINAGE MANUAL. THE MAXIMUM PEAK FLOW RATE FROM THE 1-YR 24 HOUR STORM IN THE POST DEVELOPMENT CONDITION IS 1.65 CFS.

HYDRAFLOW IS USED TO DETERMINE THE PROPOSED SITE RUNOFF TREATED BY THE RAIN GARDENS. THE PROPOSED SITE RUNOFF IS 1.62 CFS, MEETING THE MAXIMUM PEAK FLOW RATE REQUIREMENT.

$$Q_{\text{Developed}} = \text{I.F.} \times \left( \frac{Q_{\text{Pre-Developed}} \times \text{RV}_{\text{Pre-Developed}}}{\text{RV}_{\text{Developed}}} \right)$$

$$Q_{\text{Developed}} = .90 \left( \frac{1.840 \text{ cfs} \times 3829 \text{ cuft}}{3829 \text{ cuft}} \right)$$

$$Q_{\text{Developed}} = 1.65 \text{ cfs}$$

Where:

I.F. (Improvement Factor) = 0.8 for sites > 1 acre LDA or 0.9 for sites ≤ 1 acre LDA  
Q<sub>Developed</sub> = the allowable peak flow rate of runoff from the developed site for the 1-yr 24-hour storm.  
RV<sub>developed</sub> = the volume of runoff from the site in the developed condition for the 1-yr 24-hour storm.  
Q<sub>Pre-Developed</sub> = the peak flow rate of runoff from the site in the pre-developed condition for the 1-yr 24-hour storm.  
RV<sub>Pre-Developed</sub> = the volume of runoff from the site in pre-developed condition for the 1-yr 24-hour storm.  
Q<sub>Forest</sub> = the peak flow rate of runoff from the site in a forested condition for the 1-yr 24-hour storm.  
RV<sub>Forest</sub> = the volume of runoff from the site in a forested condition for the 1-yr 24-hour storm.

Hydrograph Summary Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2020

| Hyd. No. | Hydrograph type (origin) | Peak flow (cfs) | Time interval (min) | Time to Peak (min) | Hyd. volume (cuft) | Inflow hyd(s) | Maximum elevation (ft) | Total strge used (cuft) | Hydrograph Description              |
|----------|--------------------------|-----------------|---------------------|--------------------|--------------------|---------------|------------------------|-------------------------|-------------------------------------|
| 1        | SCS Runoff               | 1.840           | 2                   | 716                | 3,829              | -----         | -----                  | -----                   | Existing Total Site Drainage        |
| 2        | SCS Runoff               | 1.840           | 2                   | 716                | 3,829              | -----         | -----                  | -----                   | Proposed Site Drainage - No Treatme |
| 3        | SCS Runoff               | 1.622           | 2                   | 716                | 3,307              | -----         | -----                  | -----                   | Proposed Site Drainage - Treated    |

Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® 2019 by Autodesk, Inc. v2020

Thursday, 01 / 30 / 2020

Hyd. No. 1

Existing Total Site Drainage

|                 |              |                    |              |
|-----------------|--------------|--------------------|--------------|
| Hydrograph type | = SCS Runoff | Peak discharge     | = 1.840 cfs  |
| Storm frequency | = 1 yrs      | Time to peak       | = 716 min    |
| Time interval   | = 2 min      | Hyd. volume        | = 3,829 cuft |
| Drainage area   | = 0.630 ac   | Curve number       | = 92         |
| Basin Slope     | = 0.0 %      | Hydraulic length   | = 0 ft       |
| Tc method       | = User       | Time of conc. (Tc) | = 5.00 min   |
| Total precip.   | = 2.60 in    | Distribution       | = Type II    |
| Storm duration  | = 24 hrs     | Shape factor       | = 484        |

URBAN BIORETENTION SIZING

MINIMUM SIZING REQUIREMENT WAS DETERMINED PER VA DCR STORMWATER DESIGN SPECIFICATION 9, APPENDIX 9-A

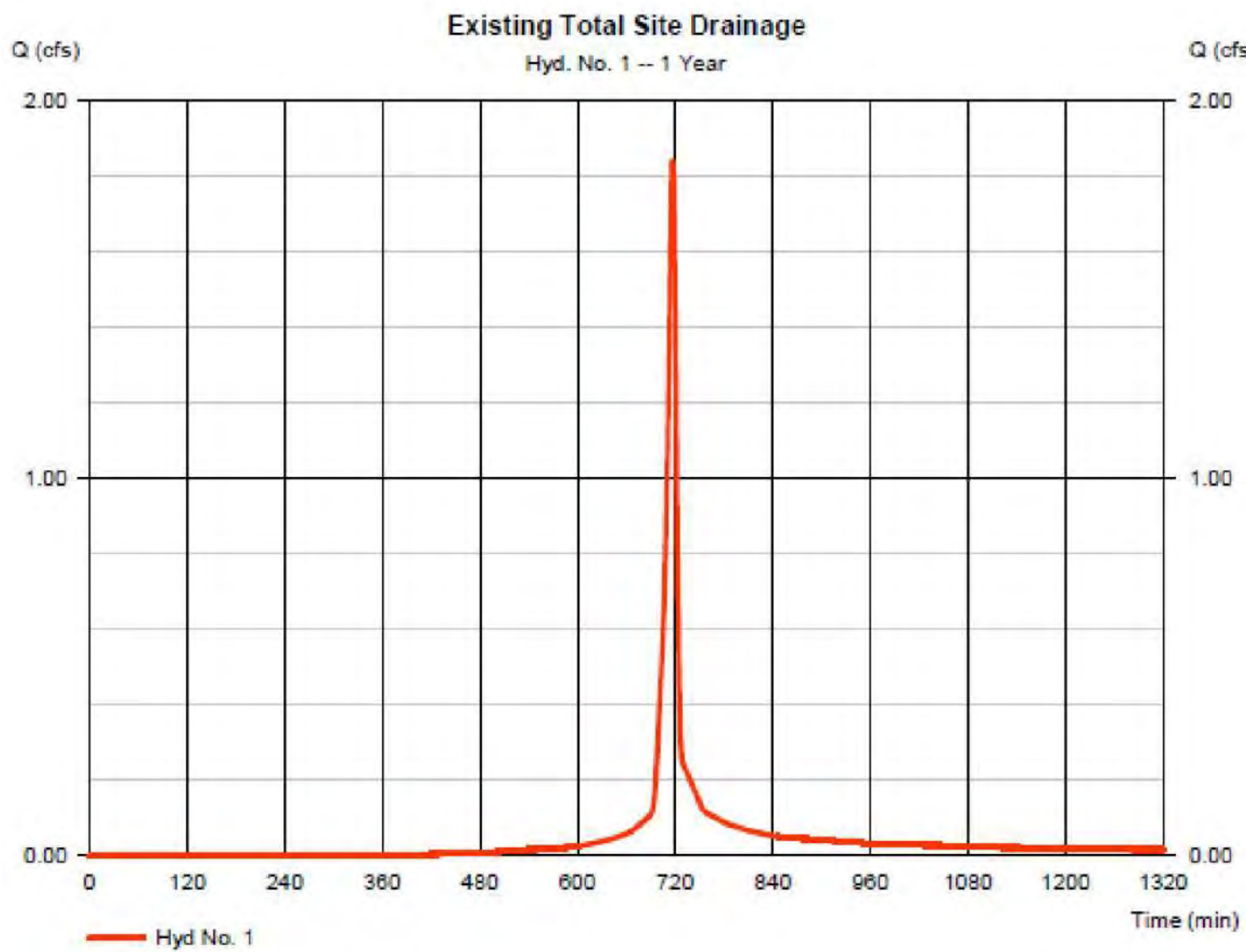
PROPOSED BMP'S MEET THE MINIMUM SIZING REQUIREMENT

$$Tv_{BMP} = [(1)(R_v)(A)/12]$$

$$Tv_{BMP} = [(1)(1664)(.63)/12]$$

$$Tv_{BMP} = 87.36 \text{ cuft}$$

$$\frac{87.36 \text{ ft}^3}{4} = 21.84 \text{ ft}^3 \text{ per BMP}$$



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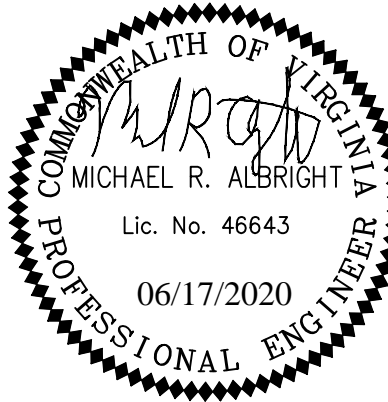
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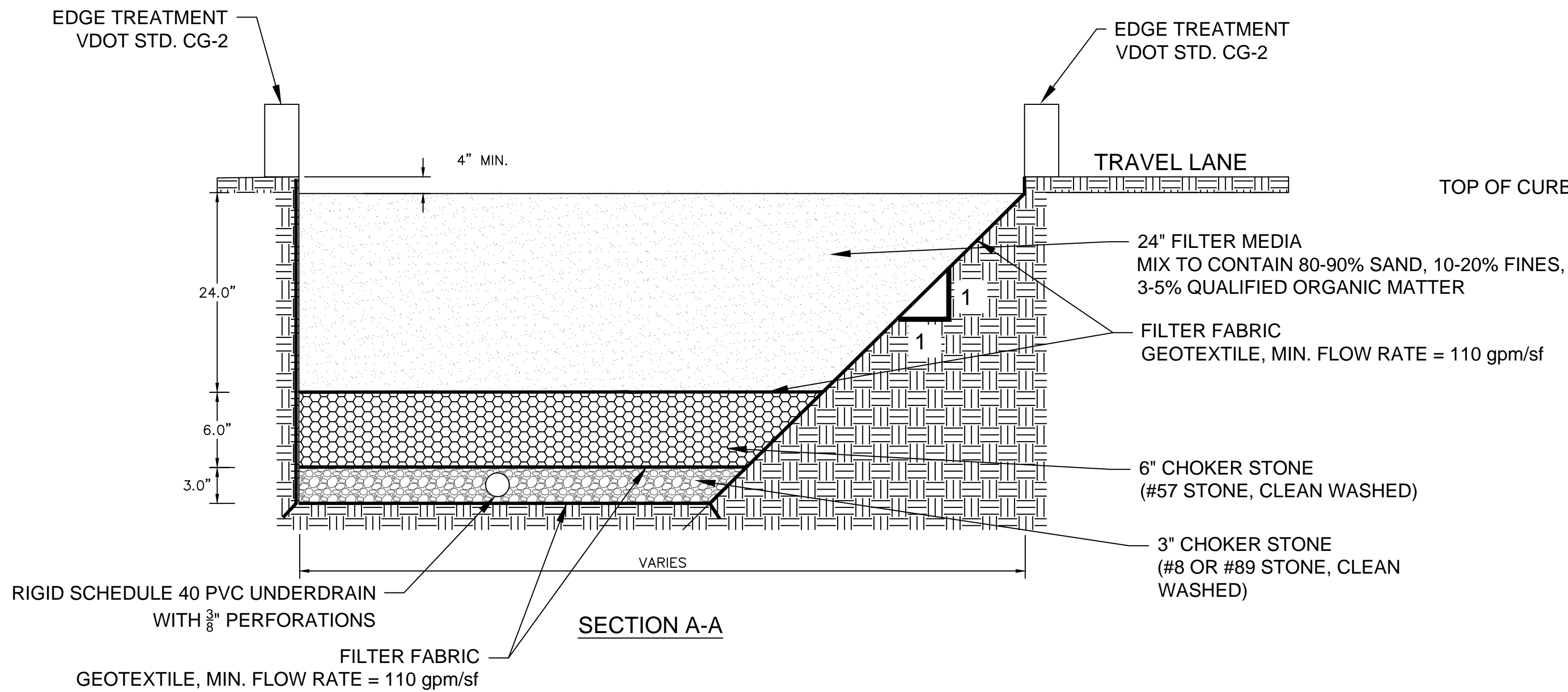
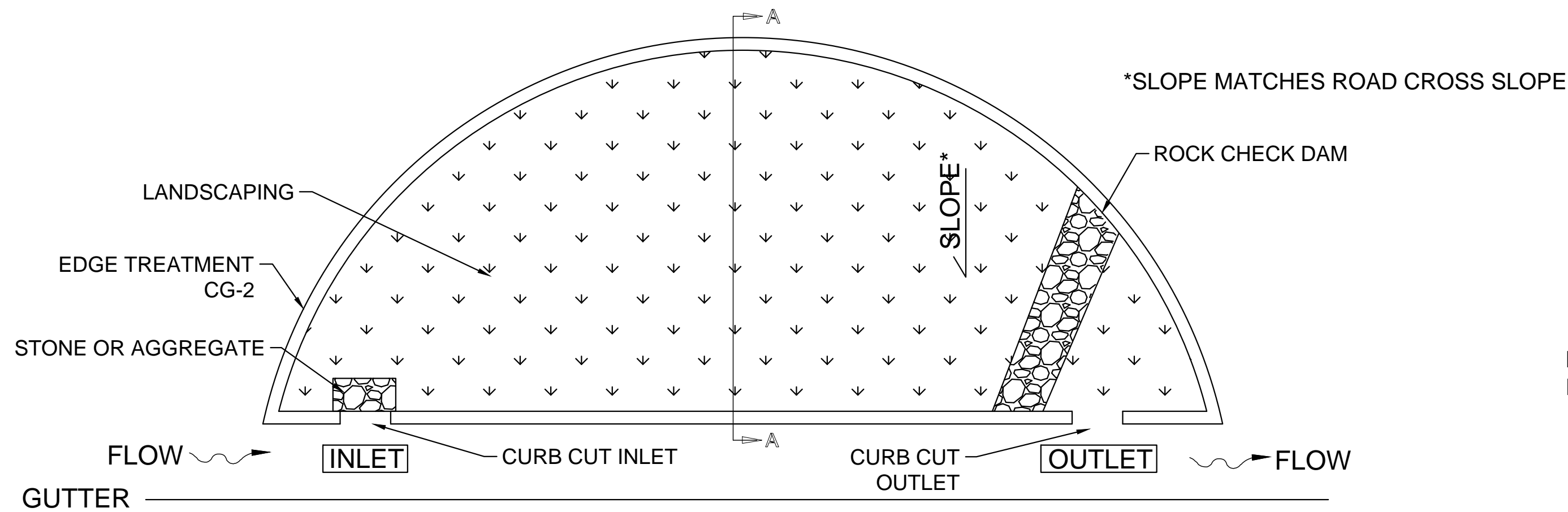
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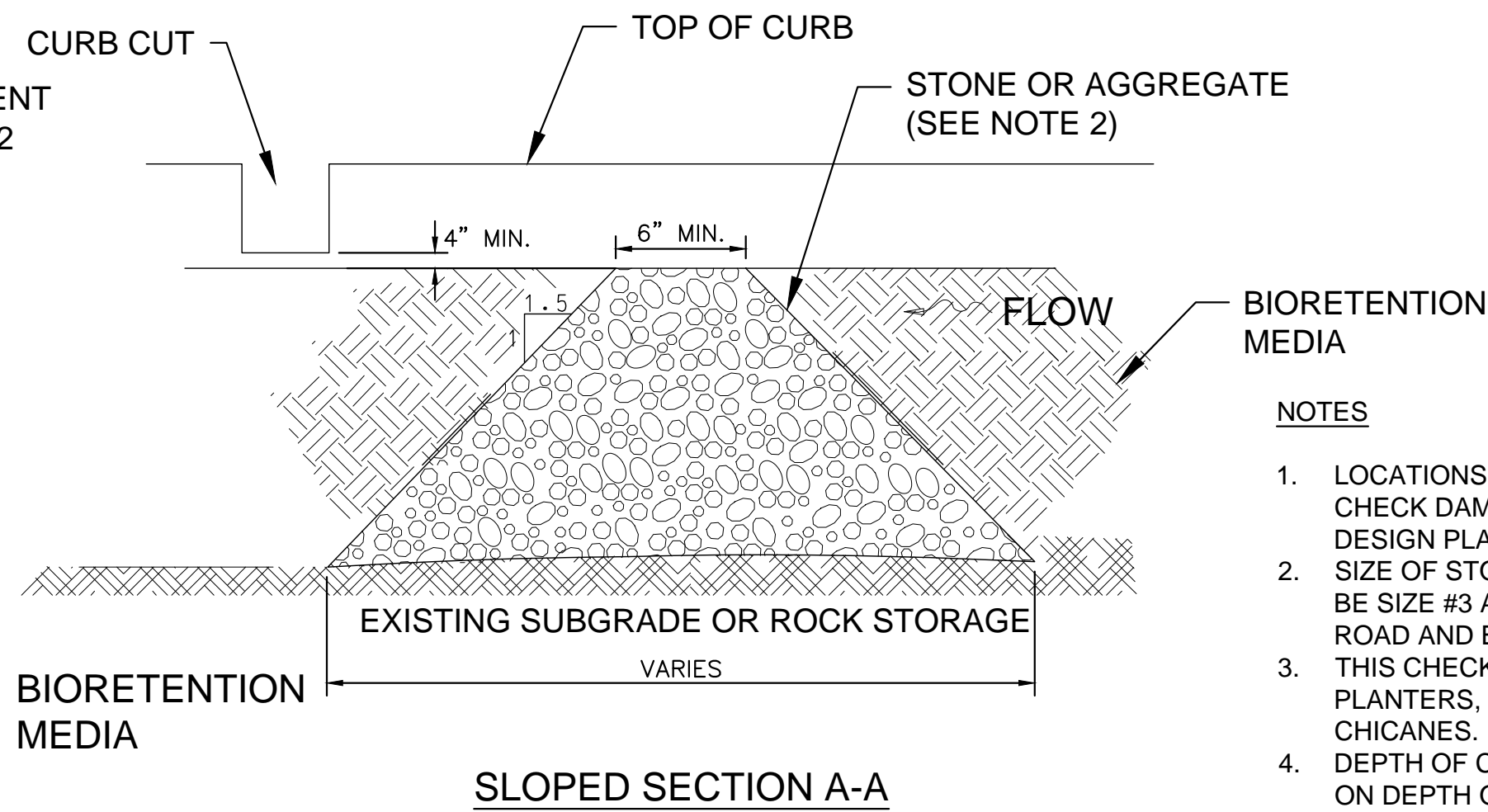
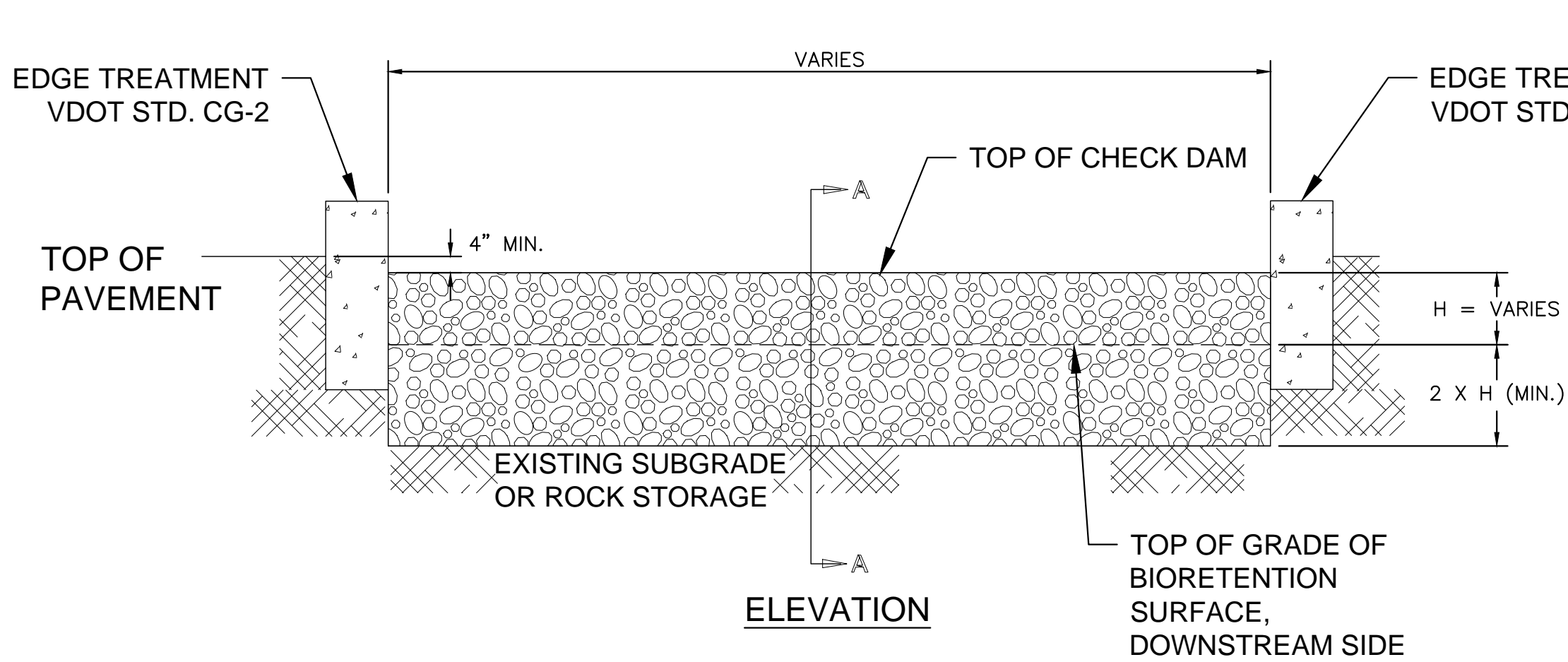


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## RAIN GARDEN



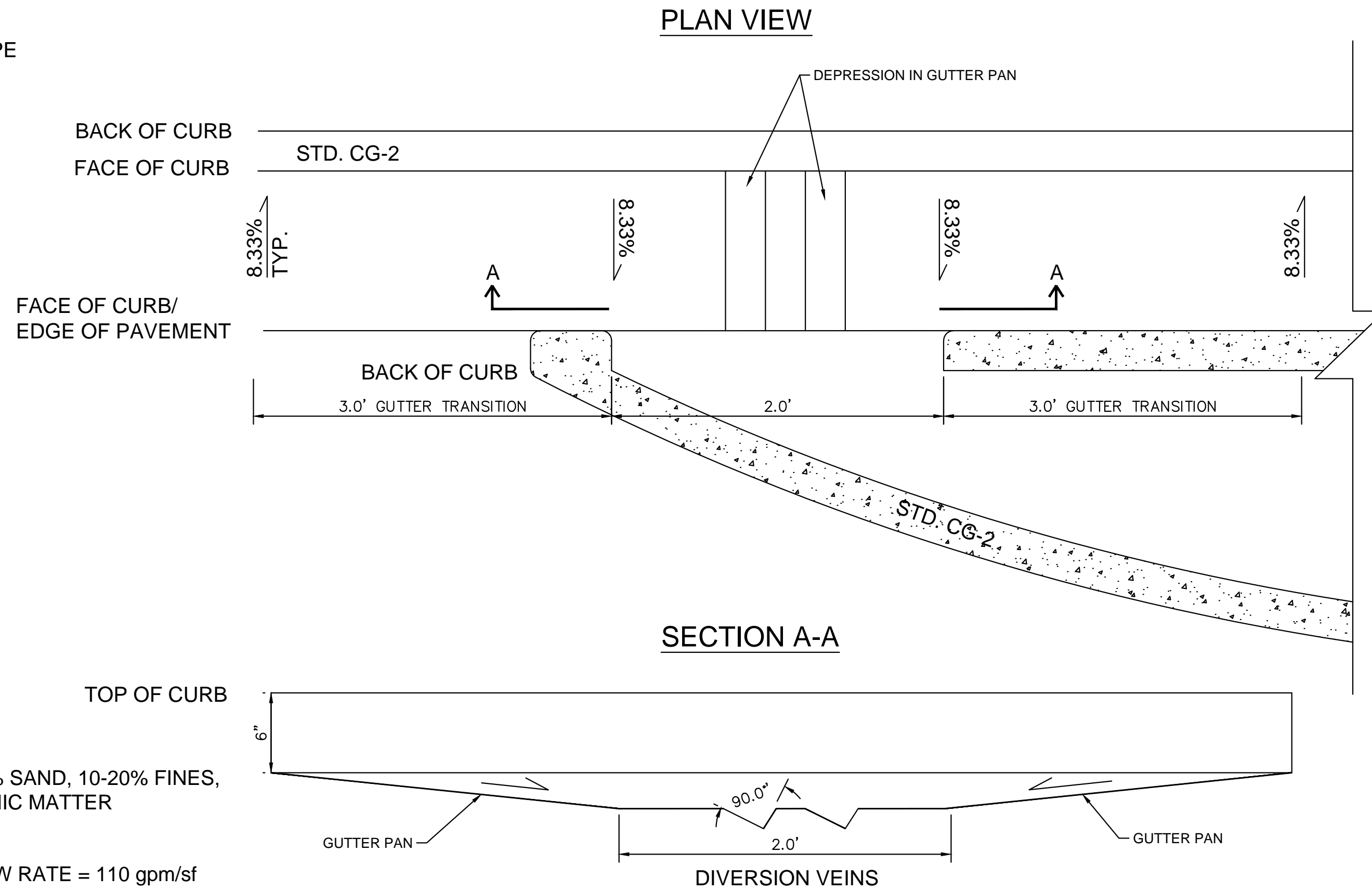
## ROCK CHECK DAM



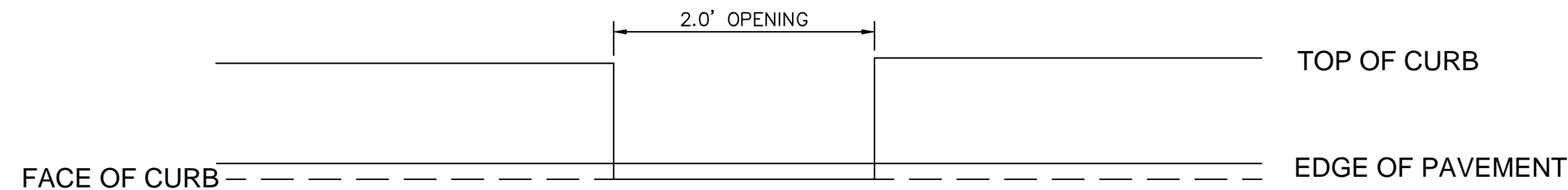
### NOTES

1. LOCATIONS, HEIGHTS, AND WIDTHS OF CHECK DAMS TO BE SPECIFIED IN THE DESIGN PLANS.
2. SIZE OF STONE OR AGGREGATE APRON TO BE SIZE #3 AS SPECIFIED IN VDOT 2016 ROAD AND BRIDGE SPECIFICATIONS.
3. THIS CHECK DAM MAY BE USED IN PLANTERS, CURB EXTENSIONS OR CHICANES.
4. DEPTH OF CHECK DAM VARIES DEPENDING ON DEPTH OF THE FACILITY.

## CURB CUT INLET



## CURB CUT OUTLET



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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
BMP DETAILS

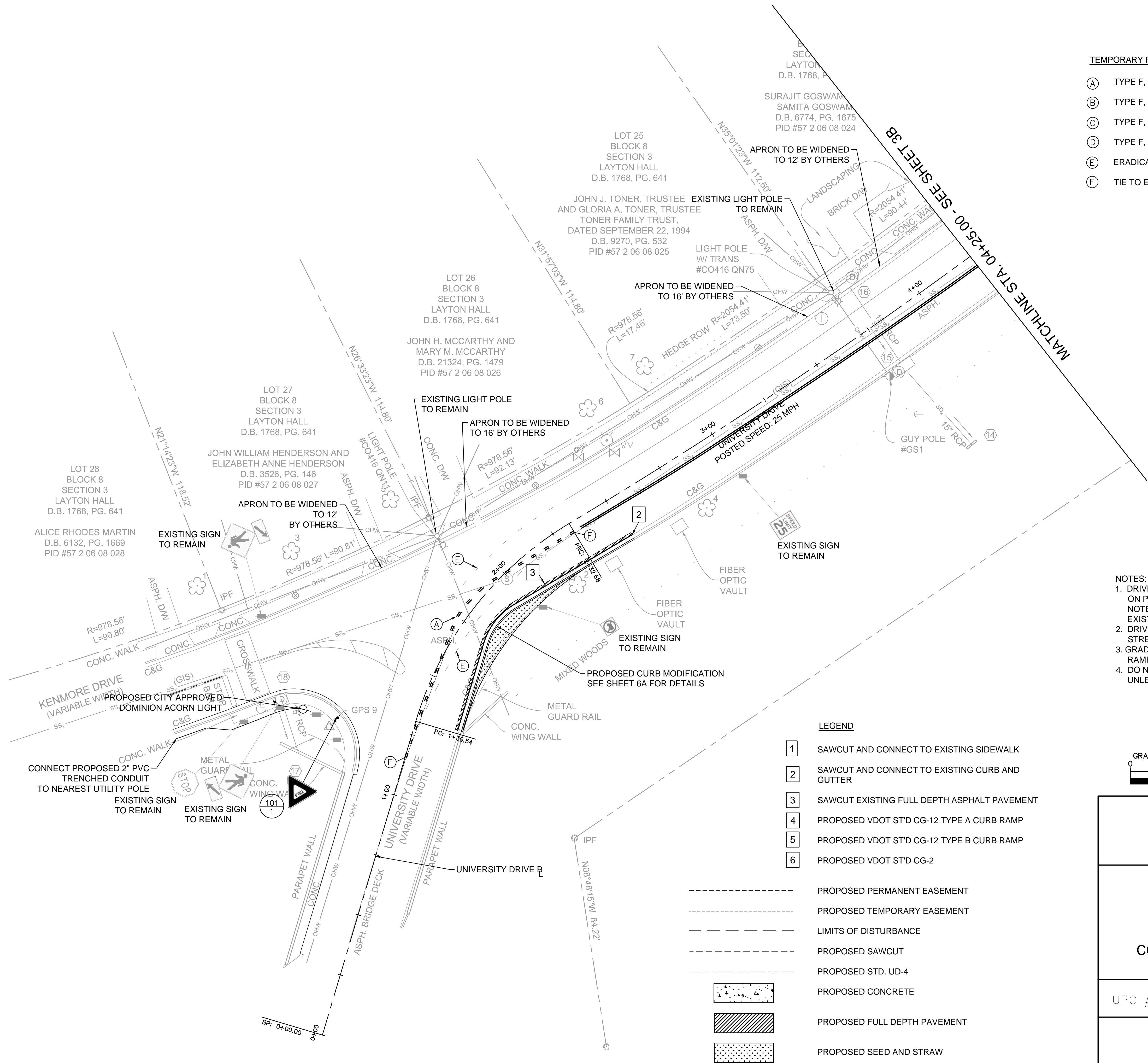
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TEMPORARY PAVEMENT MARKING LEGEND

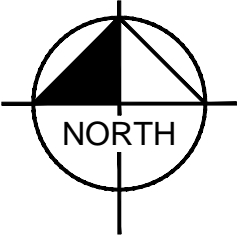
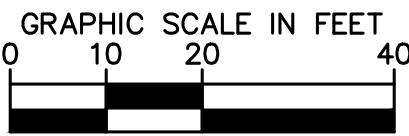
- (A) TYPE F, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE F, WHITE, 4" WIDTH
- (C) TYPE F, WHITE, 6" WIDTH
- (D) TYPE F, WHITE, 24" WIDTH
- (E) ERADICATE EXISTING PAVEMENT MARKINGS
- (F) TIE TO EXISTING PAVEMENT MARKINGS

- NOTES:
1. DRIVEWAYS TO BE WIDENED TO DIMENSIONS NOTED ON PLAN AND ACCORDING TO DETAIL ON GENERAL NOTES SHEET 2B. DRIVEWAYS TO BE REBUILT TO EXISTING FLOW LINE.
  2. DRIVEWAYS TO BE WIDENED WITH HIGH EARLY STRENGTH CONCRETE
  3. GRADE AROUND PROPOSED ASPHALT AND CURB RAMPS TO ENSURE POSITIVE DRAINAGE.
  4. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.

LEGEND

- 1 SAWCUT AND CONNECT TO EXISTING SIDEWALK
- 2 SAWCUT AND CONNECT TO EXISTING CURB AND GUTTER
- 3 SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT
- 4 PROPOSED VDOT STD CG-12 TYPE A CURB RAMP
- 5 PROPOSED VDOT STD CG-12 TYPE B CURB RAMP
- 6 PROPOSED VDOT STD CG-2

- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW



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KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

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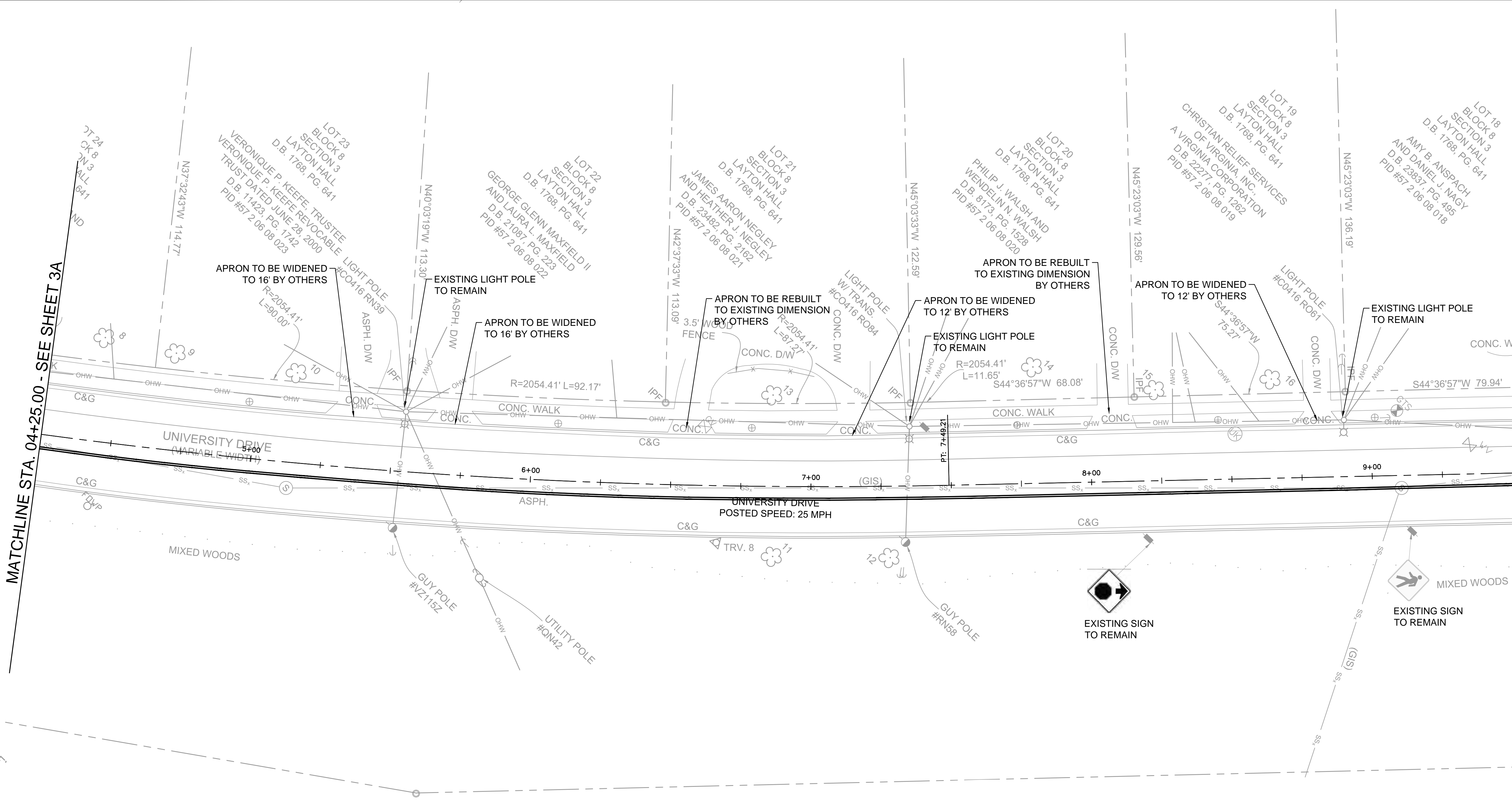
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MATCHLINE STA. 04+25.00 - SEE SHEET 3A

MATCHLINE STA. 09+50.00 - SEE SHEET 3C



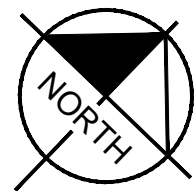
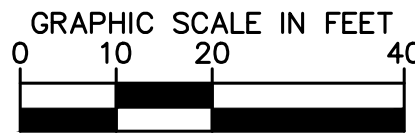
LEGEND

- 1 SAWCUT AND CONNECT TO EXISTING SIDEWALK
- 2 SAWCUT AND CONNECT TO EXISTING CURB AND GUTTER
- 3 SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT
- 4 PROPOSED VDOT STD CG-12 TYPE A CURB RAMP
- 5 PROPOSED VDOT STD CG-12 TYPE B CURB RAMP
- 6 PROPOSED VDOT STD CG-2

- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW

TEMPORARY PAVEMENT MARKING LEGEND

- A TYPE F, YELLOW, 4" WIDTH, 4" SPACE
- B TYPE F, WHITE, 4" WIDTH
- C TYPE F, WHITE, 6" WIDTH
- D TYPE F, WHITE, 24" WIDTH
- E ERADICATE EXISTING PAVEMENT MARKINGS
- F TIE TO EXISTING PAVEMENT MARKINGS



- NOTES:
1. DRIVEWAYS TO BE WIDENED TO DIMENSIONS NOTED ON PLAN AND ACCORDING TO DETAIL ON GENERAL NOTES SHEET 2B. DRIVEWAYS TO BE REBUILT TO EXISTING FLOW LINE.
  2. DRIVEWAYS TO BE WIDENED WITH HIGH EARLY STRENGTH CONCRETE
  3. GRADE AROUND PROPOSED ASPHALT AND CURB RAMPS TO ENSURE POSITIVE DRAINAGE.
  4. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.



CITY OF FAIRFAX

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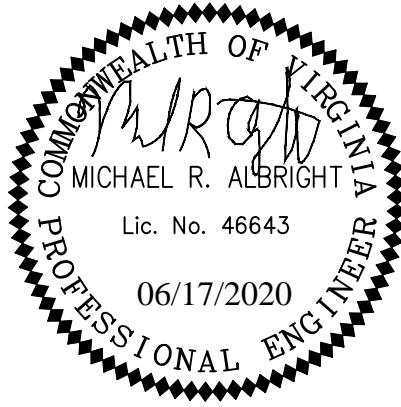
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Phone: 703-385-7889

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20191  
Phone: 703-674-1300  
Fax: 703-674-1350

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

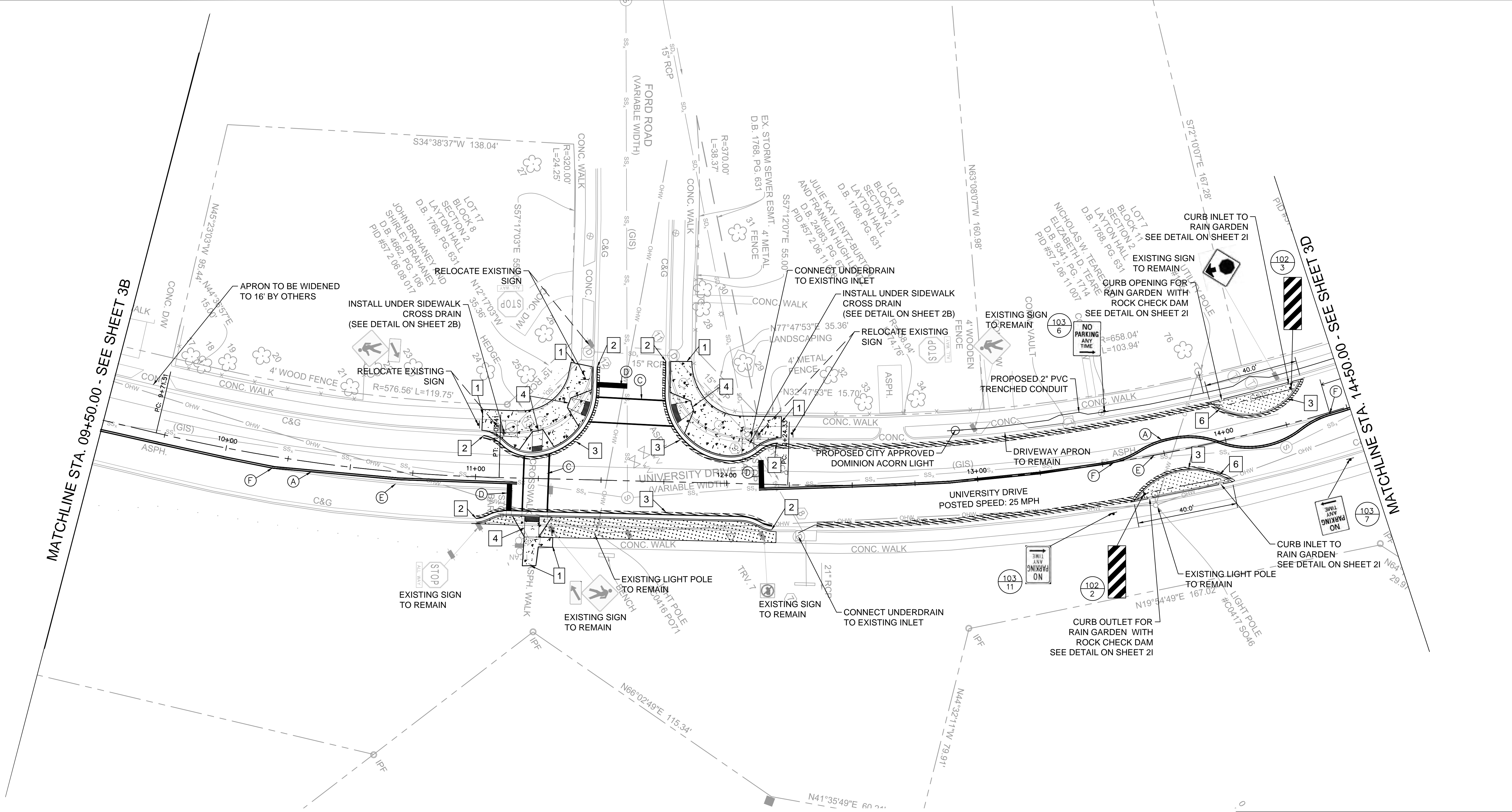
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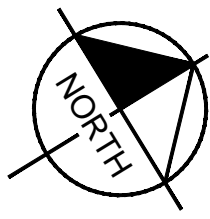
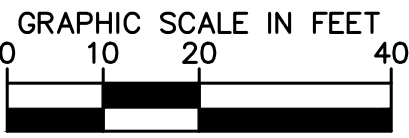
LEGEND

- 1 SAWCUT AND CONNECT TO EXISTING SIDEWALK
- 2 SAWCUT AND CONNECT TO EXISTING CURB AND GUTTER
- 3 SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT
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- 5 PROPOSED VDOT ST'D CG-12 TYPE B CURB RAMP
- 6 PROPOSED VDOT ST'D CG-2

- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW

TEMPORARY PAVEMENT MARKING LEGEND

- A TYPE F, YELLOW, 4" WIDTH, 4" SPACE
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- D TYPE F, WHITE, 24" WIDTH
- E ERADICATE EXISTING PAVEMENT MARKINGS
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  4. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.



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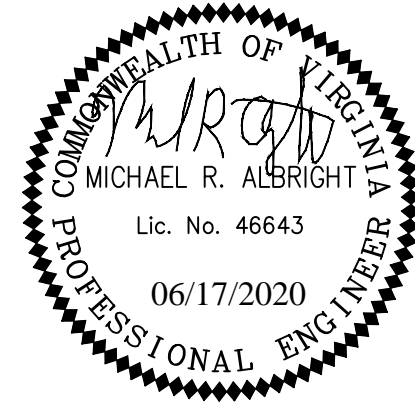
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

UPC # 113121

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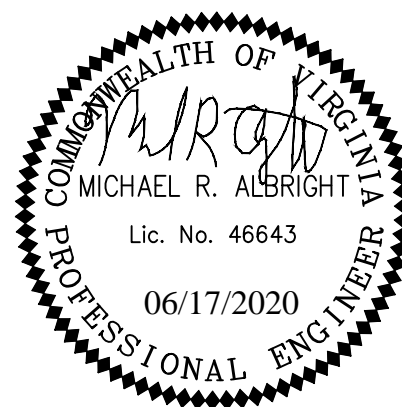
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# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

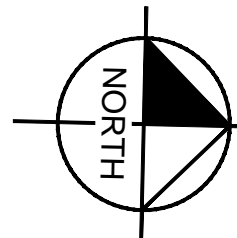
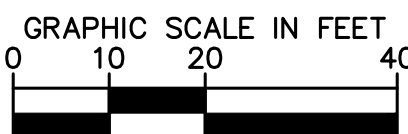
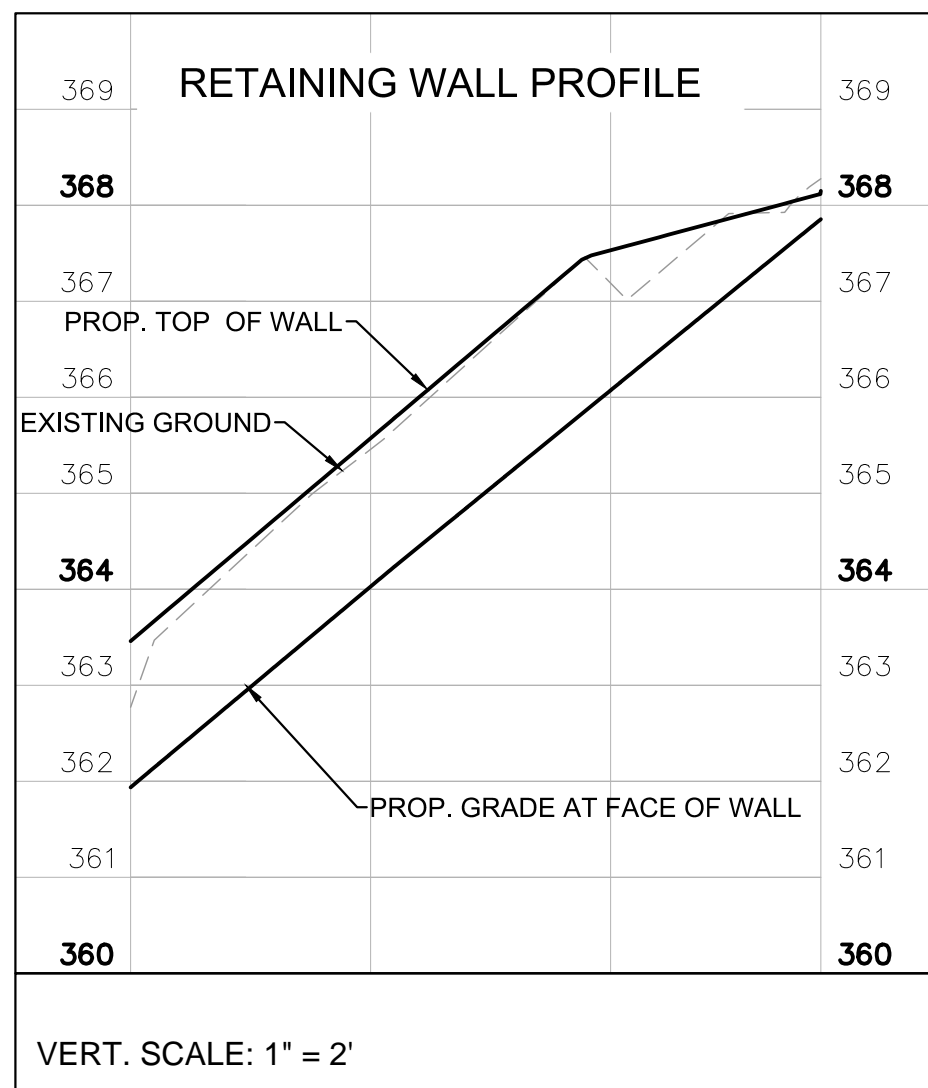
### CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

UPC # 113121

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- |   | <u>LEGEND</u>                                  |
|---|--|
| 1 | SAWCUT AND CONNECT TO EXISTING SIDEWALK        |
| 2 | SAWCUT AND CONNECT TO EXISTING CURB AND GUTTER |
| 3 | SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT    |
| 4 | PROPOSED VDOT ST'D CG-12 TYPE A CURB RAMP      |
| 5 | PROPOSED VDOT ST'D CG-12 TYPE B CURB RAMP      |
| 6 | PROPOSED VDOT ST'D CG-2                        |

- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW

## TEMPORARY PAVEMENT MARKING LEGEND

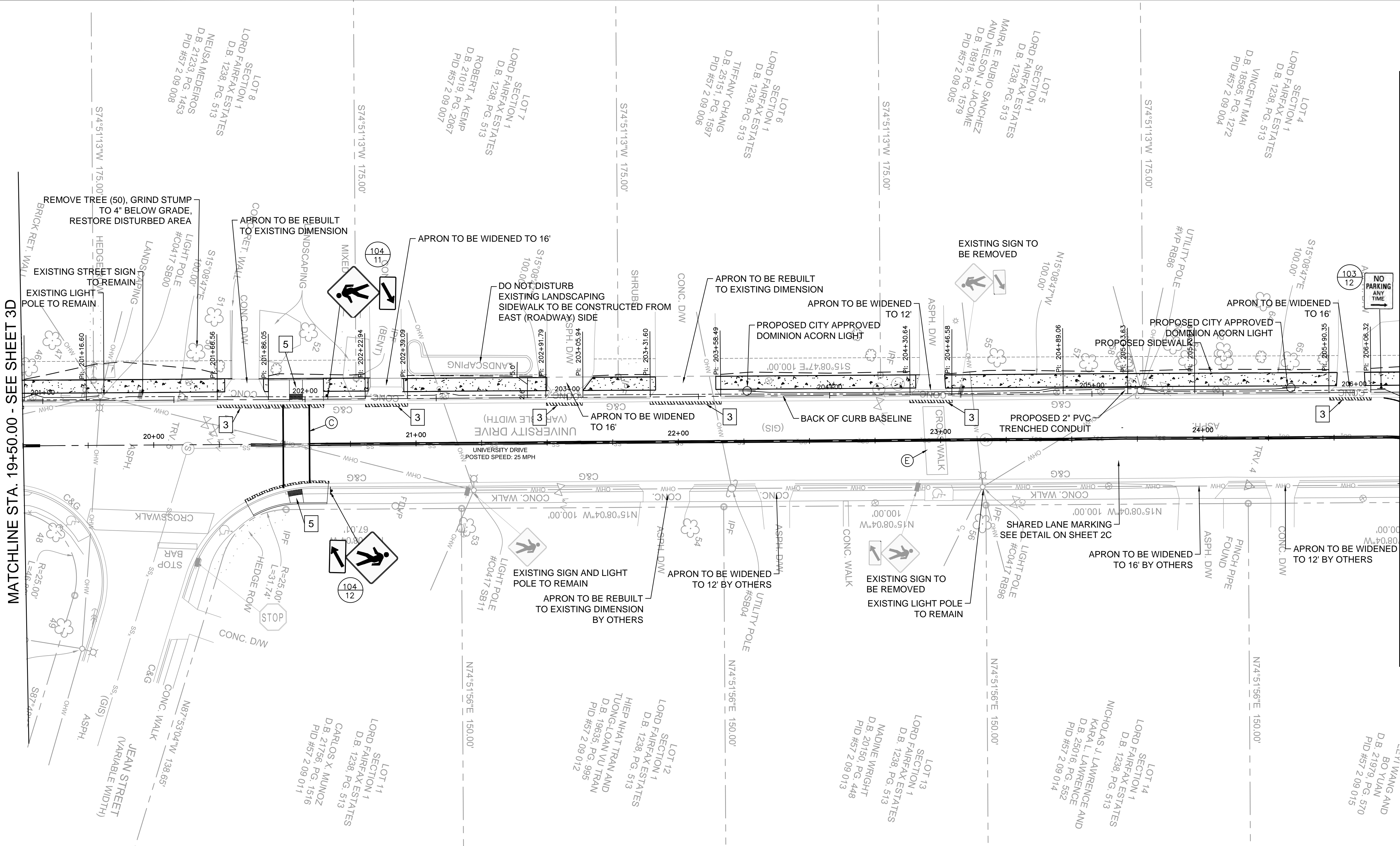
- (A) TYPE F, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE F, WHITE, 4" WIDTH
- (C) TYPE F, WHITE, 6" WIDTH
- (D) TYPE F, WHITE, 24" WIDTH
- (E) ERADICATE EXISTING PAVEMENT MARKINGS
- (F) TIE TO EXISTING PAVEMENT MARKINGS

NOTES:

1. DRIVEWAYS TO BE WIDENED TO DIMENSIONS NOTED ON PLAN AND ACCORDING TO DETAIL ON GENERAL NOTES SHEET 2B. DRIVEWAYS TO BE REBUILT TO EXISTING FLOW LINE.
2. DRIVEWAYS TO BE WIDENED WITH HIGH EARLY STRENGTH CONCRETE
3. GRADE AROUND PROPOSED ASPHALT AND CURB RAMPS TO ENSURE POSITIVE DRAINAGE.
4. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.



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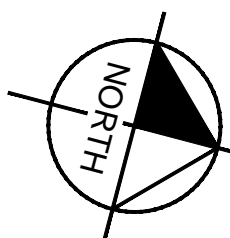
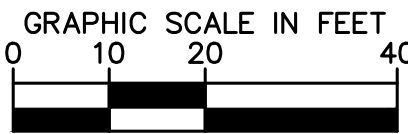
MATCHLINE STA. 24+75.00 - SEE SHEET 3F

LEGEND

- 1 SAWCUT AND CONNECT TO EXISTING SIDEWALK
- 2 SAWCUT AND CONNECT TO EXISTING CURB AND GUTTER
- 3 SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT
- 4 PROPOSED VDOT STD CG-12 TYPE A CURB RAMP
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- 6 PROPOSED VDOT STD CG-2
- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW

TEMPORARY PAVEMENT MARKING LEGEND

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- C TYPE F, WHITE, 6" WIDTH
- D TYPE F, WHITE, 24" WIDTH
- E ERADICATE EXISTING PAVEMENT MARKINGS
- F TIE TO EXISTING PAVEMENT MARKINGS



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DEPARTMENT OF PUBLIC WORKS

UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

UPC # 113121

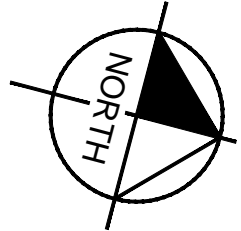
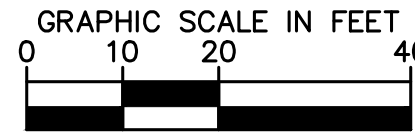
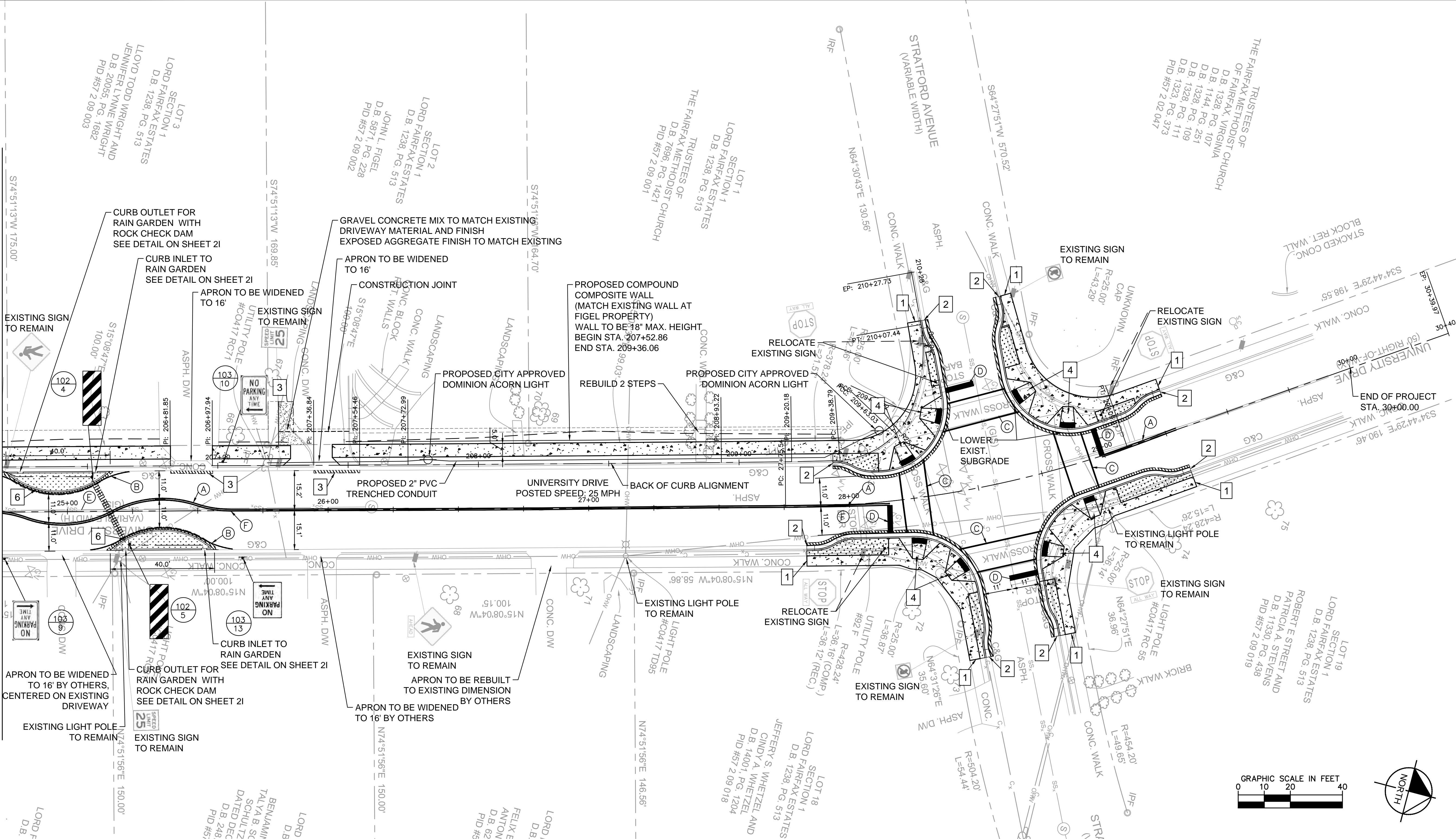
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LEGEND

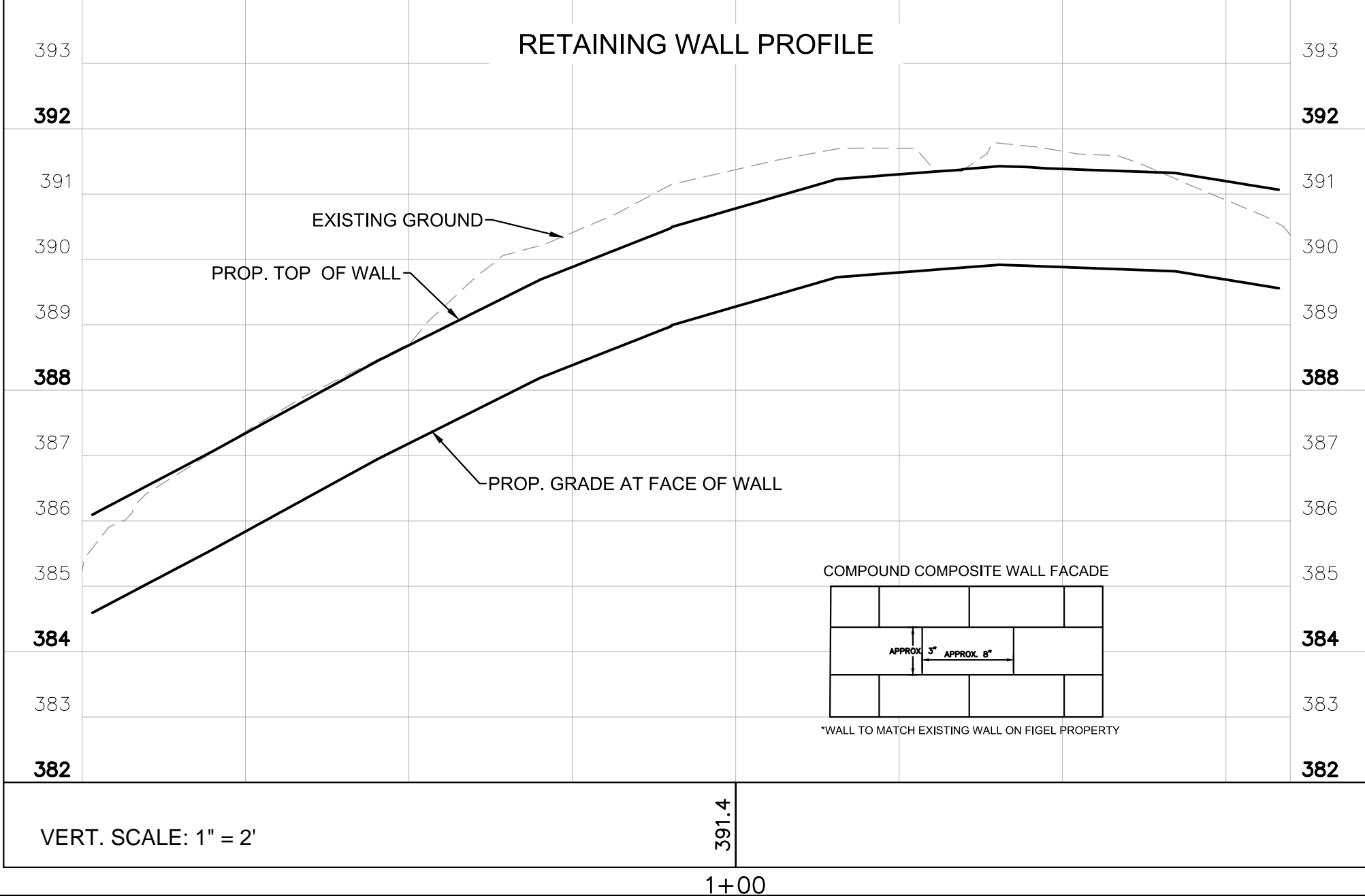
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- 3 SAWCUT EXISTING FULL DEPTH ASPHALT PAVEMENT
- 4 PROPOSED VDOT ST'D CG-12 TYPE A CURB RAMP
- 5 PROPOSED VDOT ST'D CG-12 TYPE B CURB RAMP
- 6 PROPOSED VDOT ST'D CG-2

- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE
- PROPOSED SAWCUT
- PROPOSED STD. UD-4
- PROPOSED CONCRETE
- PROPOSED FULL DEPTH PAVEMENT
- PROPOSED SEED AND STRAW

TEMPORARY PAVEMENT MARKING LEGEND

- (A) TYPE F, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE F, WHITE, 4" WIDTH
- (C) TYPE F, WHITE, 6" WIDTH
- (D) TYPE F, WHITE, 24" WIDTH
- (E) ERADICATE EXISTING PAVEMENT MARKINGS
- (F) TIE TO EXISTING PAVEMENT MARKINGS

- NOTES:
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CITY OF FAIRFAX, VIRGINIA  
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CONSTRUCTION PLAN AND INTERIM PAVEMENT MARKINGS

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

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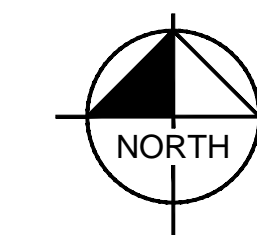
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CITY OF FAIRFAX

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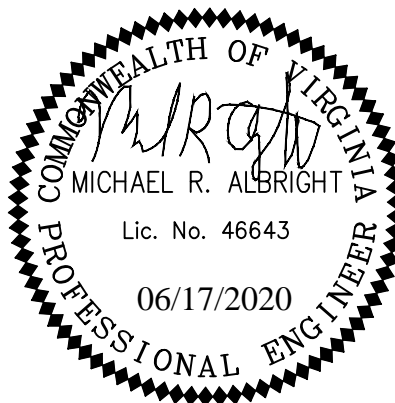
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# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

### FINAL PAVEMENT MARKING PLAN

UPC # 113121

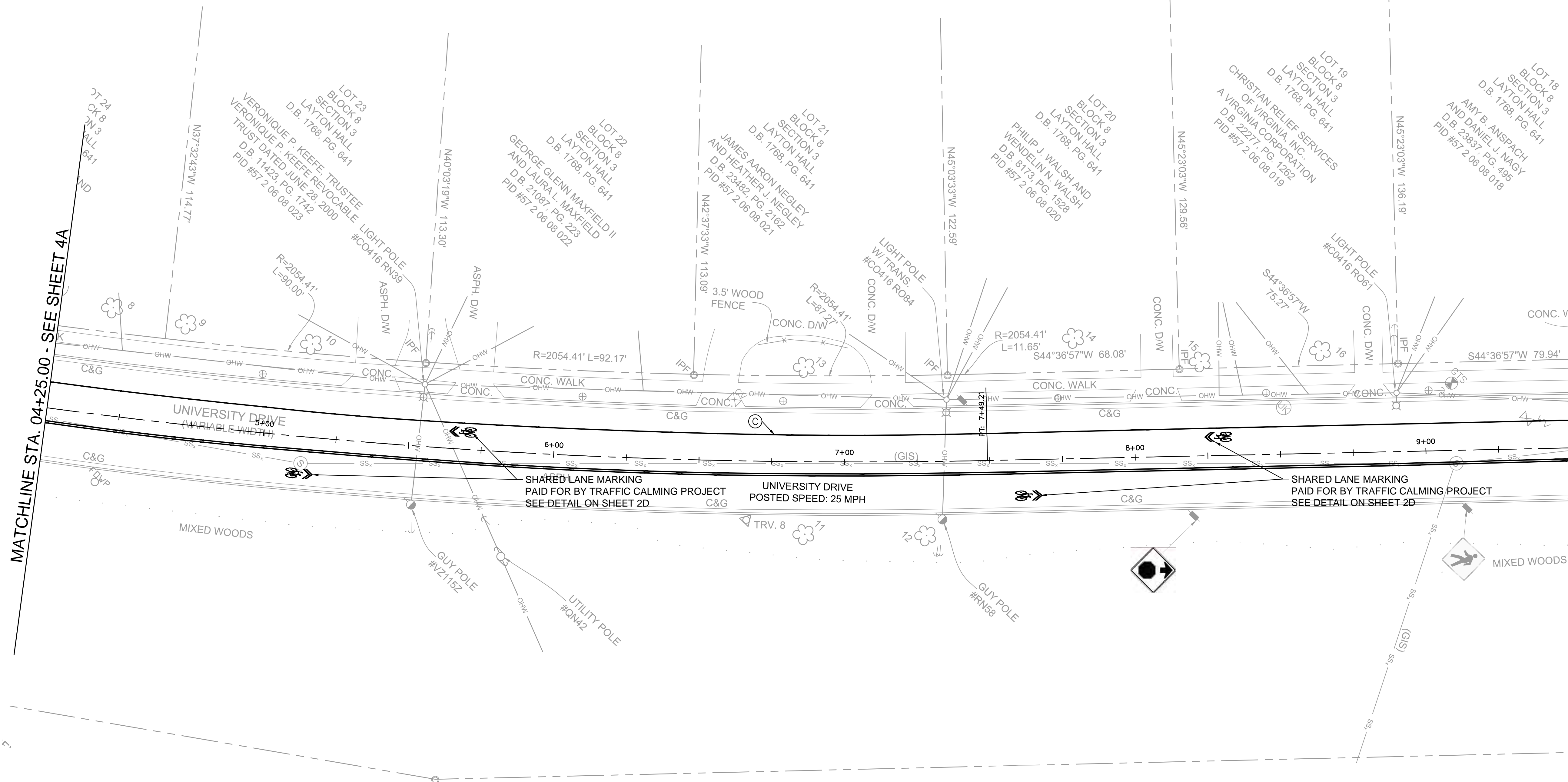
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MATCHLINE STA. 04+25.00 - SEE SHEET 4A

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LEGEND

----- PROPOSED PERMANENT EASEMENT

----- PROPOSED TEMPORARY EASEMENT

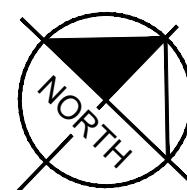
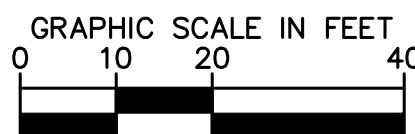
----- LIMITS OF DISTURBANCE

### PAVEMENT MARKING LEGEND

- (A) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE, 24" LENGTH
- (C) TYPE B, CLASS 1, WHITE, 4" WIDTH
- (D) TYPE B, CLASS 1, WHITE, 4" WIDTH, 24" LENGTH
- (E) TYPE B, CLASS 1, WHITE, 6" WIDTH
- (F) TYPE B, CLASS 1, WHITE, 24" WIDTH
- (G) TYPE B, CLASS 1, WHITE, 24" WIDTH, 24" SPACE
- (H) ERADICATE EXISTING PAVEMENT MARKINGS
- (I) TIE TO EXISTING PAVEMENT MARKINGS

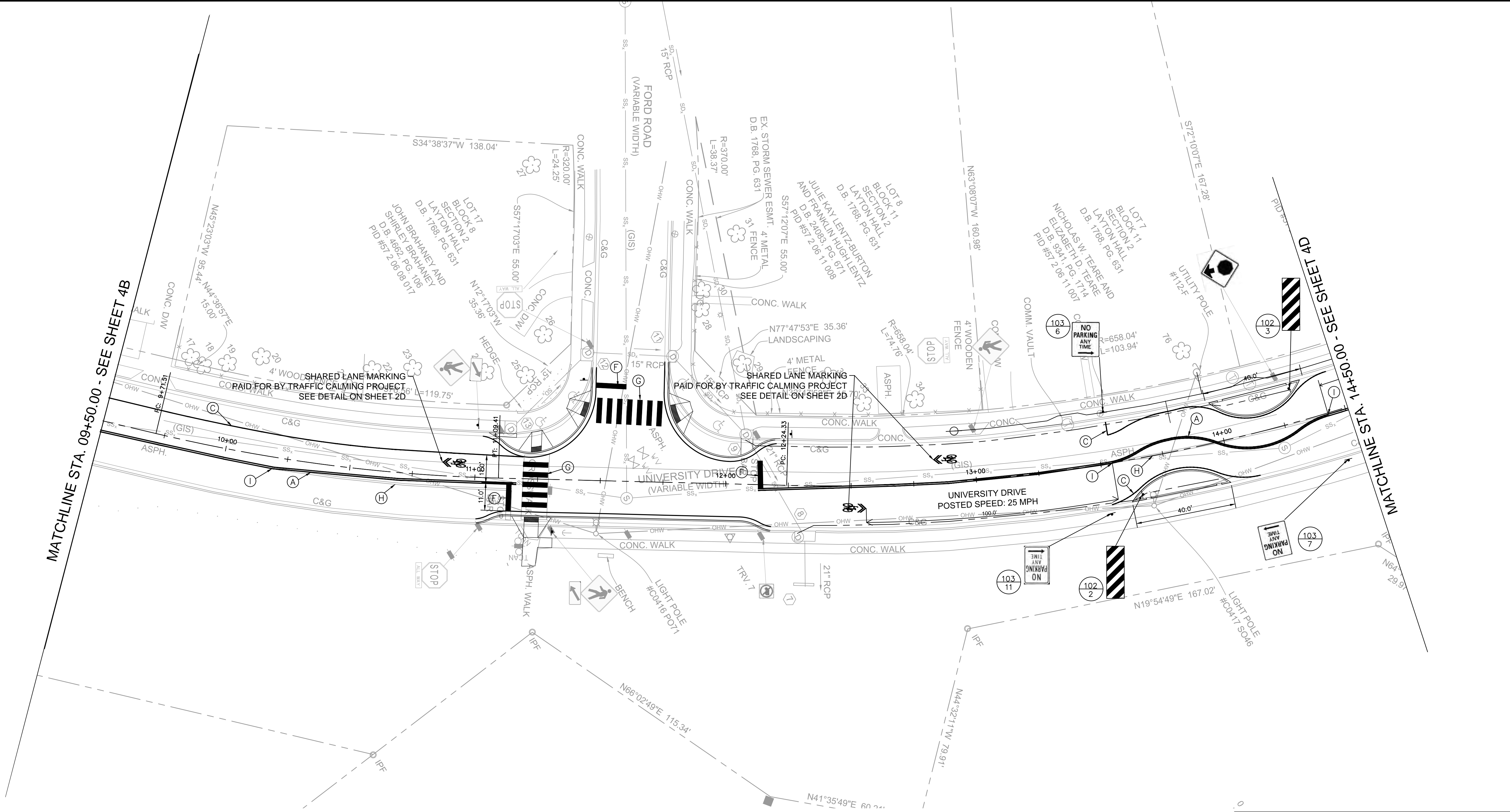
## NOTES

1. ALL PAVEMENT MARKINGS TO BE INSTALLED BY OTHERS.
2. ALL PAVEMENT MARKINGS TO BE INSTALLED AFTER COMPLETION OF ROADWAY PAVING.





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**LEGEND**

----- PROPOSED PERMANENT EASEMENT

----- PROPOSED TEMPORARY EASEMENT

--- LIMITS OF DISTURBANCE

**PAVEMENT MARKING LEGEND**

(A) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE

(B) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE, 24" LENGTH

(C) TYPE B, CLASS 1, WHITE, 4" WIDTH

(D) TYPE B, CLASS 1, WHITE, 4" WIDTH, 24" LENGTH

(E) TYPE B, CLASS 1, WHITE, 6" WIDTH

(F) TYPE B, CLASS 1, WHITE, 24" WIDTH

(G) TYPE B, CLASS 1, WHITE, 24" WIDTH, 24" SPACE

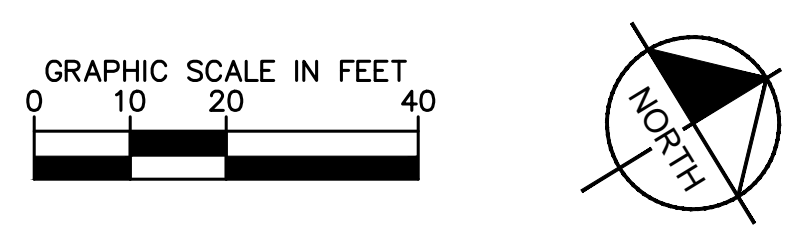
(H) ERADICATE EXISTING PAVEMENT MARKINGS


(I) TIE TO EXISTING PAVEMENT MARKINGS

**NOTES**

1. ALL PAVEMENT MARKINGS TO BE INSTALLED BY OTHERS.

2. ALL PAVEMENT MARKINGS TO BE INSTALLED AFTER COMPLETION OF ROADWAY PAVING.






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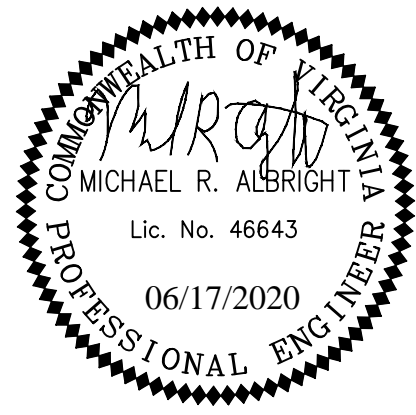
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DEPARTMENT OF PUBLIC WORKS

**UNIVERSITY DRIVE TRAFFIC CALMING**

KENMORE DRIVE TO STRATFORD AVENUE

FINAL PAVEMENT MARKING PLAN

UPC # 113121

SCALE

SEE GRAPHIC SCALE

SHEET

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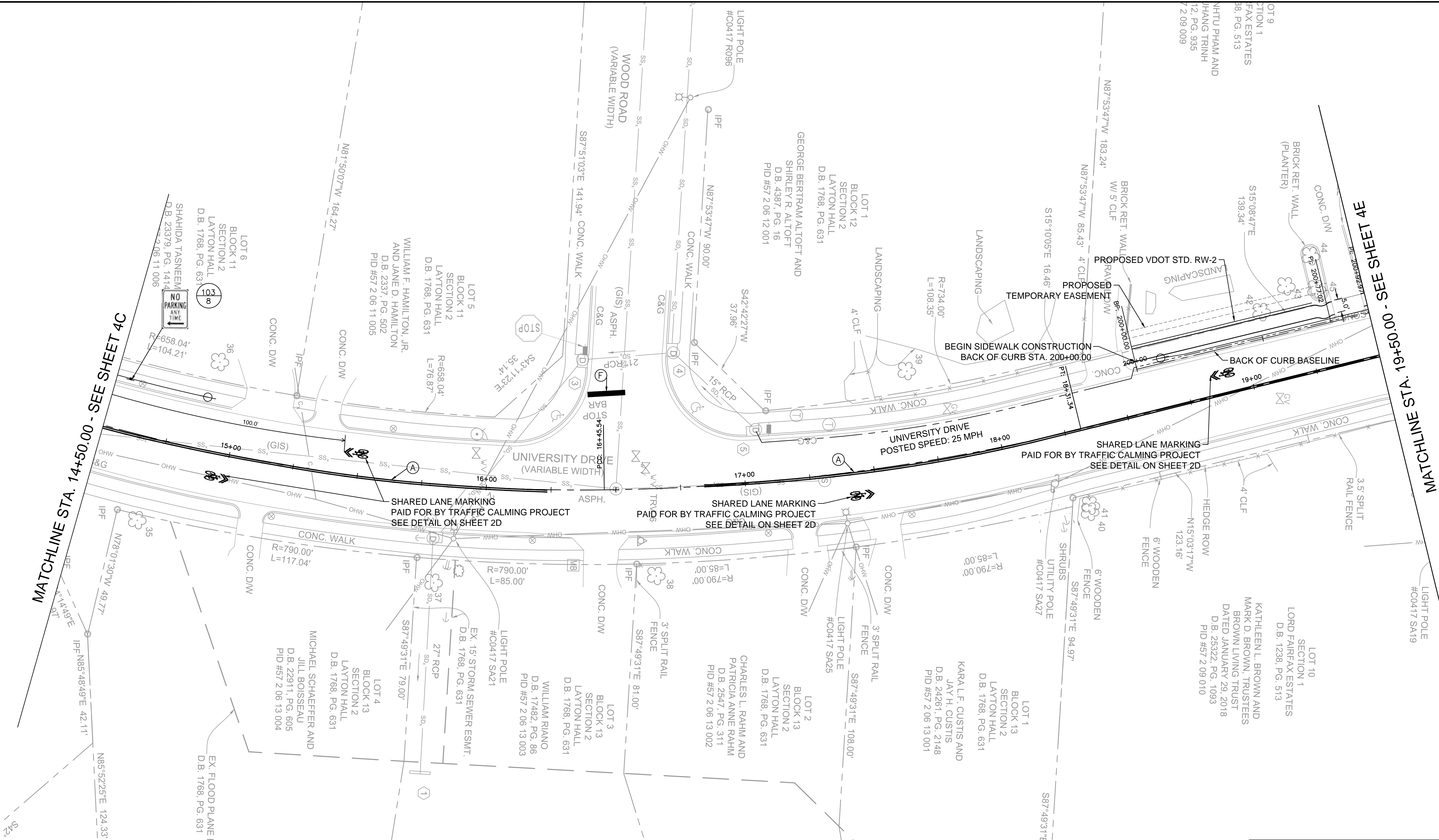
UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
FINAL PAVEMENT MARKING PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET

4D



LEGEND

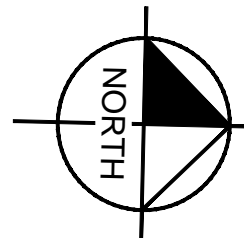
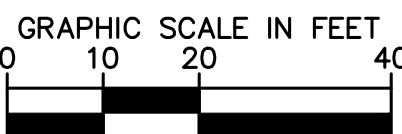
- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE

PAVEMENT MARKING LEGEND

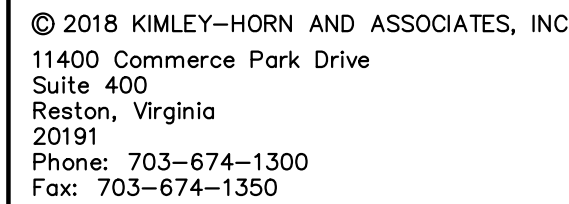
- (A) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE, 24" LENGTH
- (C) TYPE B, CLASS 1, WHITE, 4" WIDTH
- (D) TYPE B, CLASS 1, WHITE, 4" WIDTH, 24" LENGTH
- (E) TYPE B, CLASS 1, WHITE, 6" WIDTH
- (F) TYPE B, CLASS 1, WHITE, 24" WIDTH
- (G) TYPE B, CLASS 1, WHITE, 24" WIDTH, 24" SPACE
- (H) ERADICATE EXISTING PAVEMENT MARKINGS
- (I) TIE TO EXISTING PAVEMENT MARKINGS

NOTES

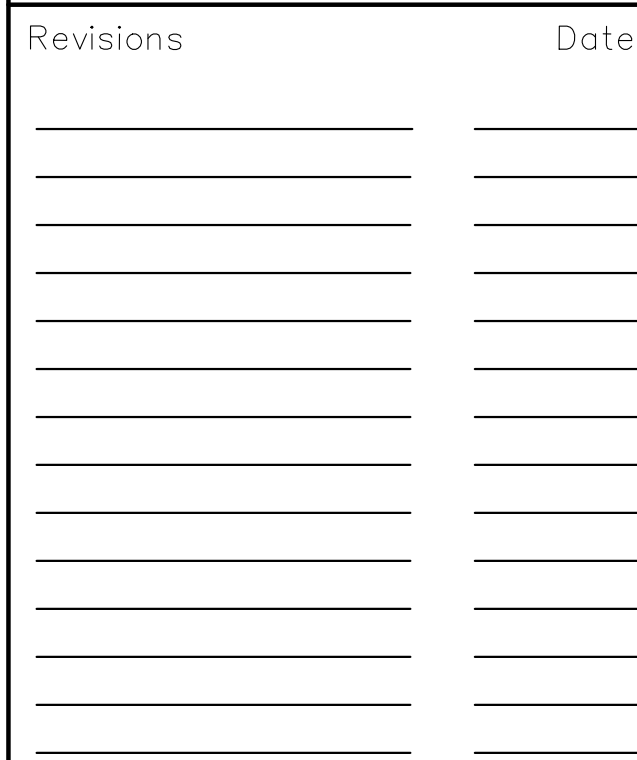
- ALL PAVEMENT MARKINGS TO BE INSTALLED BY OTHERS.
- ALL PAVEMENT MARKINGS TO BE INSTALLED AFTER COMPLETION OF ROADWAY PAVING.







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# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

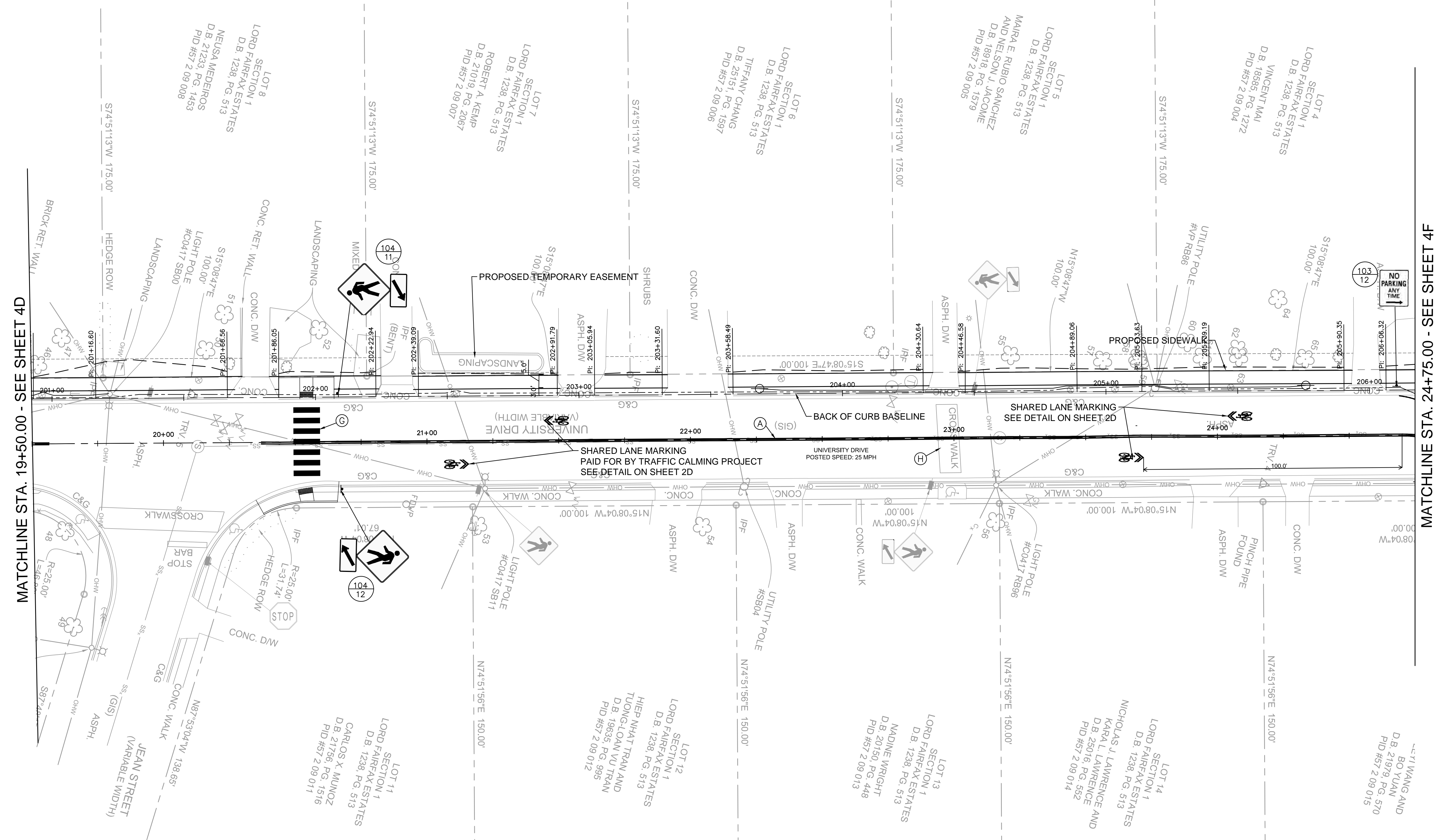
### FINAL PAVEMENT MARKING PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

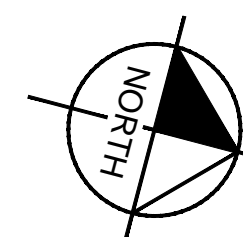
SHEET

4E



GRAPHIC SCALE IN FEET

A horizontal line with tick marks at 0, 10, 20, and 40 feet. The segment between 0 and 10 is white, 10 and 20 is black, 20 and 40 is white, and the segment after 40 is black.



### PAVEMENT MARKING LEGEND

- (A) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE, 24" LENGTH
- (C) TYPE B, CLASS 1, WHITE, 4" WIDTH
- (D) TYPE B, CLASS 1, WHITE, 4" WIDTH, 24" LENGTH
- (E) TYPE B, CLASS 1, WHITE, 6" WIDTH
- (F) TYPE B, CLASS 1, WHITE, 24" WIDTH
- (G) TYPE B, CLASS 1, WHITE, 24" WIDTH, 24" SPACE
- (H) ERADICATE EXISTING PAVEMENT MARKINGS
- (I) TIE TO EXISTING PAVEMENT MARKINGS

## NOTES

- NOTES**
1. ALL PAVEMENT MARKINGS TO BE INSTALLED BY OTHERS.
  2. ALL PAVEMENT MARKINGS TO BE INSTALLED AFTER COMPLETION OF ROADWAY PAVING.

### LEGEND

- PROPOSED PERMANENT EASEMENT  
PROPOSED TEMPORARY EASEMENT  
LIMITS OF DISTURBANCE

MATCHLINE STA. 19+50.00 - SEE SHEET 4D

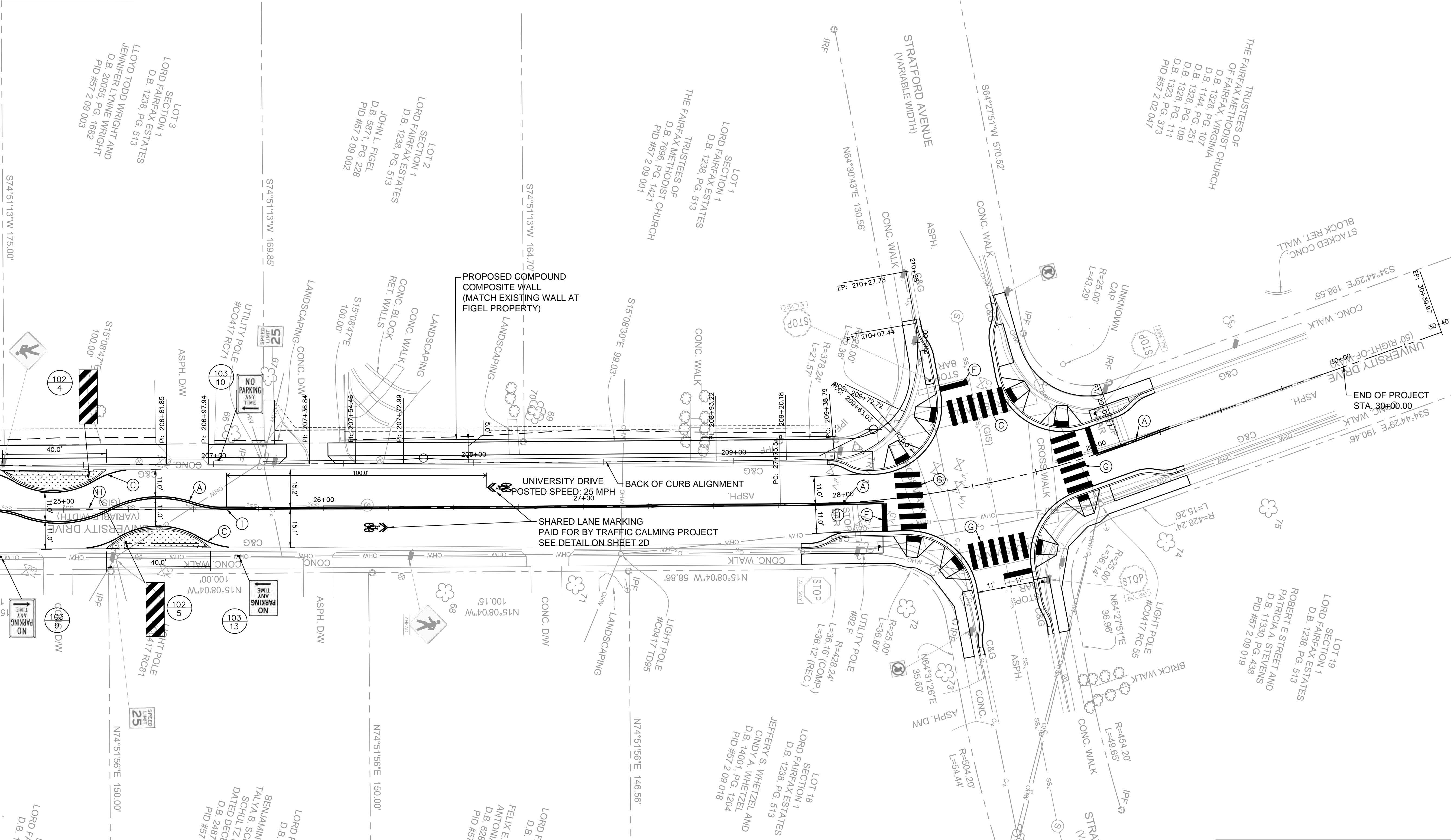
MATCHLINE STA. 24+75.00 - SEE SHEET 4F

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MATCHLINE STA. 24+75.00 - SEE SHEET 4E



LEGEND

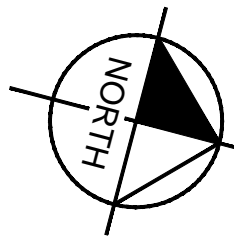
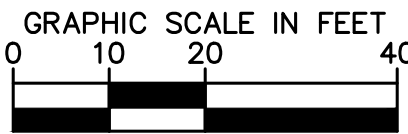
- PROPOSED PERMANENT EASEMENT
- PROPOSED TEMPORARY EASEMENT
- LIMITS OF DISTURBANCE

PAVEMENT MARKING LEGEND

- (A) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE
- (B) TYPE B, CLASS 1, YELLOW, 4" WIDTH, 4" SPACE, 24" LENGTH
- (C) TYPE B, CLASS 1, WHITE, 4" WIDTH
- (D) TYPE B, CLASS 1, WHITE, 4" WIDTH, 24" LENGTH
- (E) TYPE B, CLASS 1, WHITE, 6" WIDTH
- (F) TYPE B, CLASS 1, WHITE, 24" WIDTH
- (G) TYPE B, CLASS 1, WHITE, 24" WIDTH, 24" SPACE
- (H) ERADICATE EXISTING PAVEMENT MARKINGS
- (I) TIE TO EXISTING PAVEMENT MARKINGS

NOTES

- ALL PAVEMENT MARKINGS TO BE INSTALLED BY OTHERS.
- ALL PAVEMENT MARKINGS TO BE INSTALLED AFTER COMPLETION OF ROADWAY PAVING.



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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
FINAL PAVEMENT MARKING PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE



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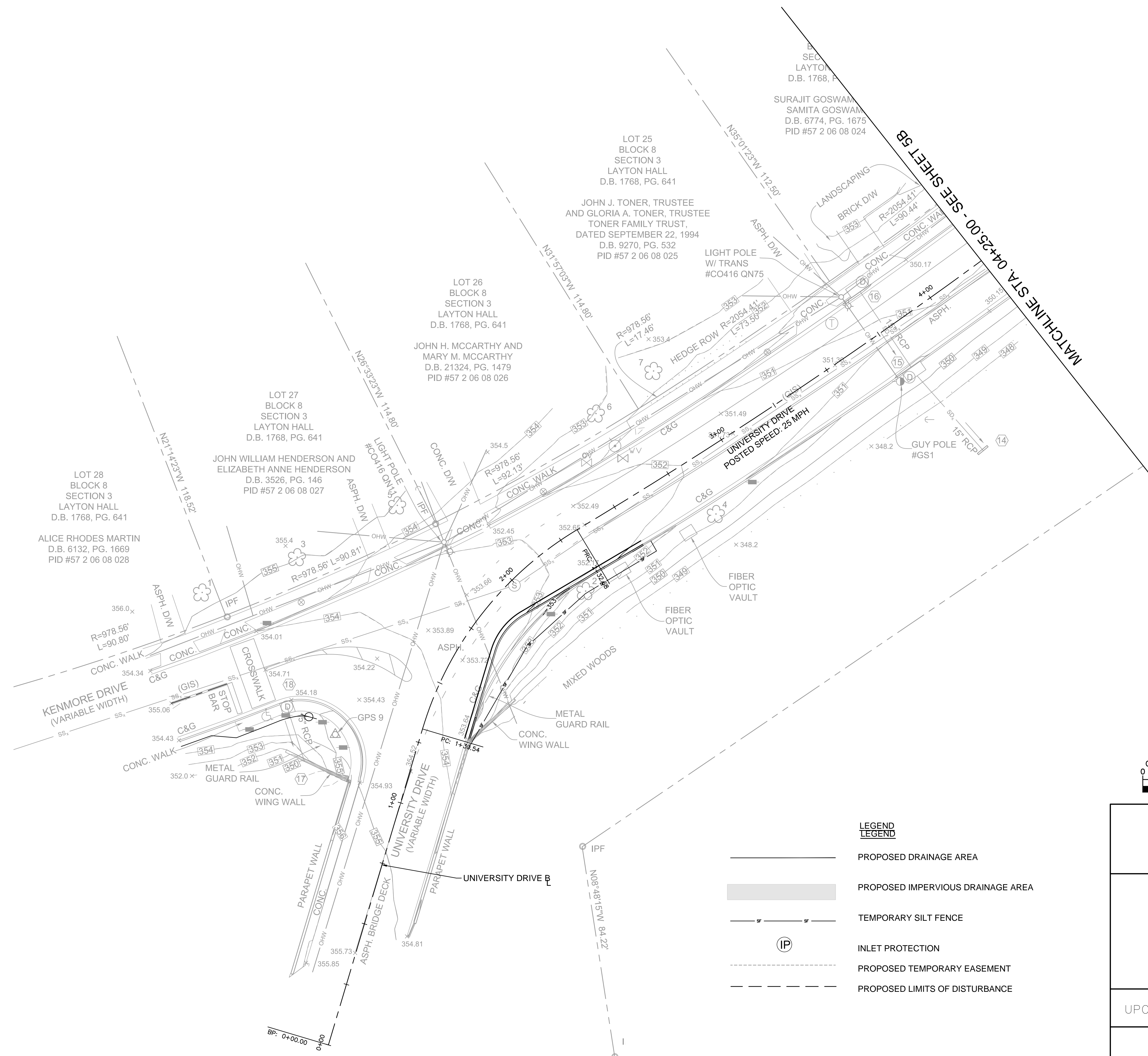
UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
EROSION AND SEDIMENT CONTROL

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

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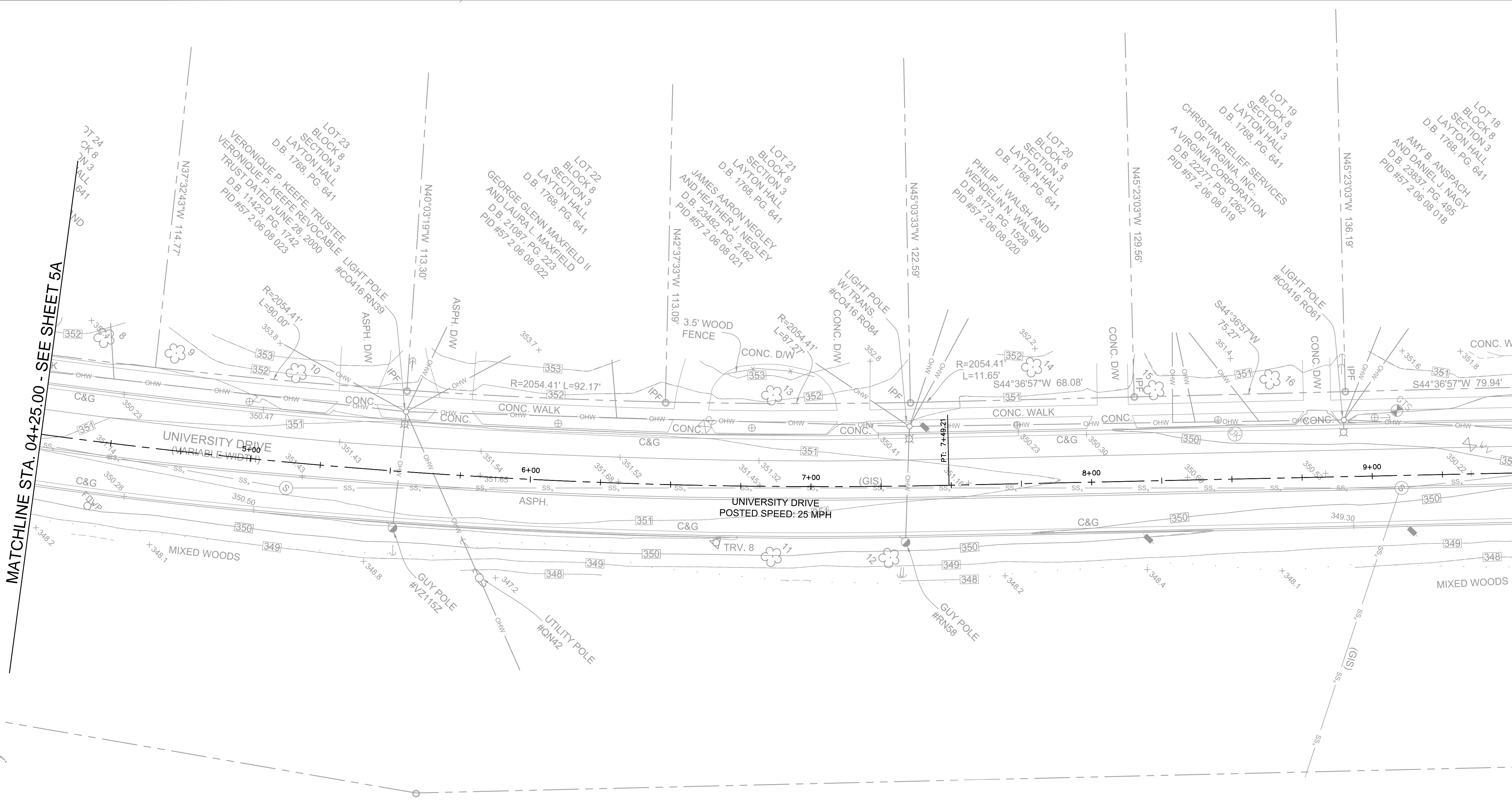


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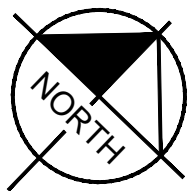
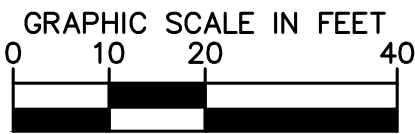
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MATCHLINE STA. 04+25.00 - SEE SHEET 5A



MATCHLINE STA. 09+50.00 - SEE SHEET 5C

- LEGEND**
- PROPOSED DRAINAGE AREA
  - PROPOSED IMPERVIOUS DRAINAGE AREA
  - TEMPORARY SILT FENCE
  - INLET PROTECTION
  - PROPOSED TEMPORARY EASEMENT
  - PROPOSED LIMITS OF DISTURBANCE



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KENMORE DRIVE TO STRATFORD AVENUE  
EROSION AND SEDIMENT CONTROL

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET  
5B



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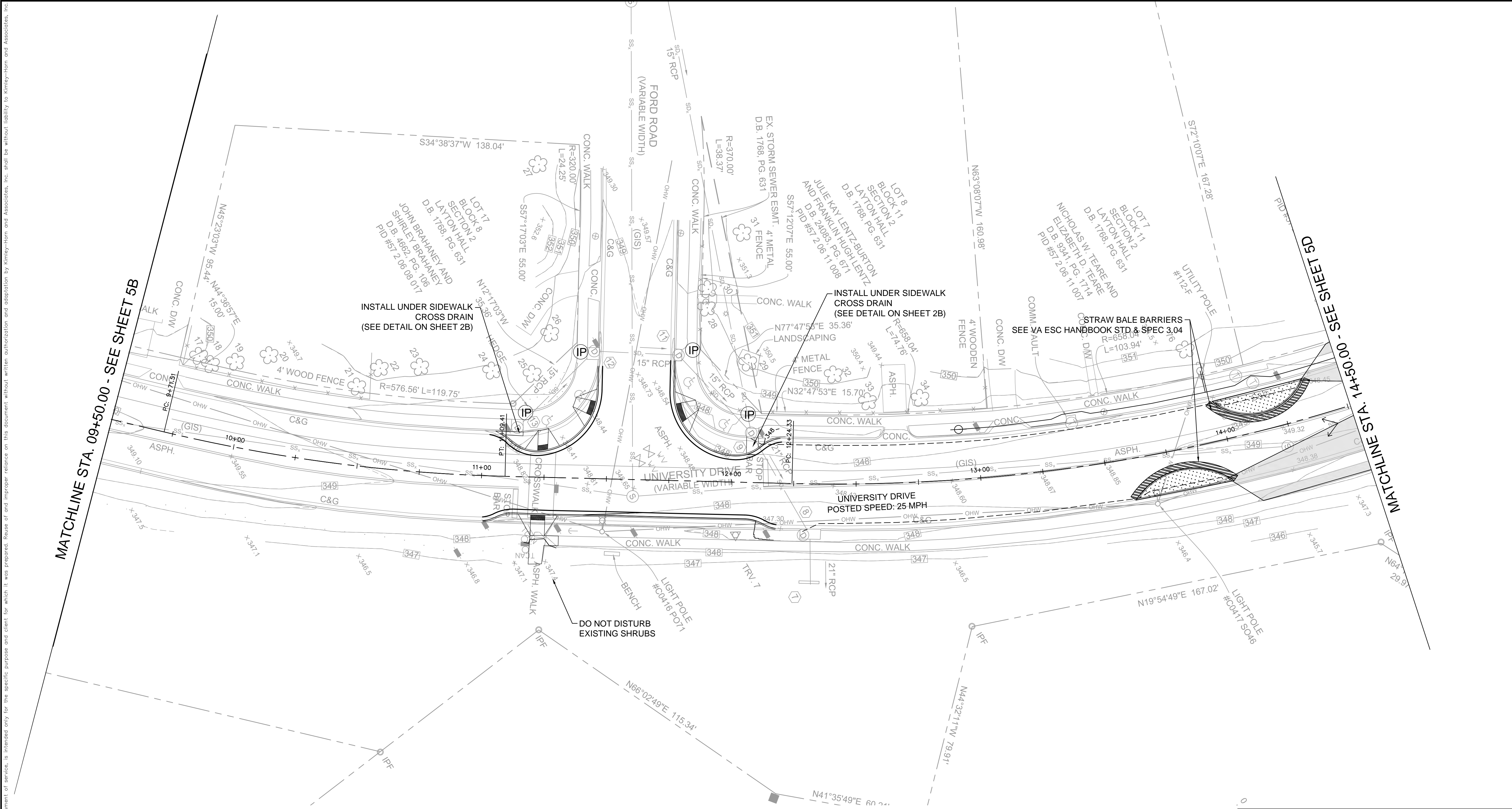


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**LEGEND**

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|  | TEMPORARY SILT FENCE              |
|  | INLET PROTECTION                  |
|  | PROPOSED TEMPORARY EASEMENT       |
|  | PROPOSED LIMITS OF DISTURBANCE    |

GRAPHIC SCALE IN FEET

0 10 20 40

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
EROSION AND SEDIMENT CONTROL

UPC # 113121

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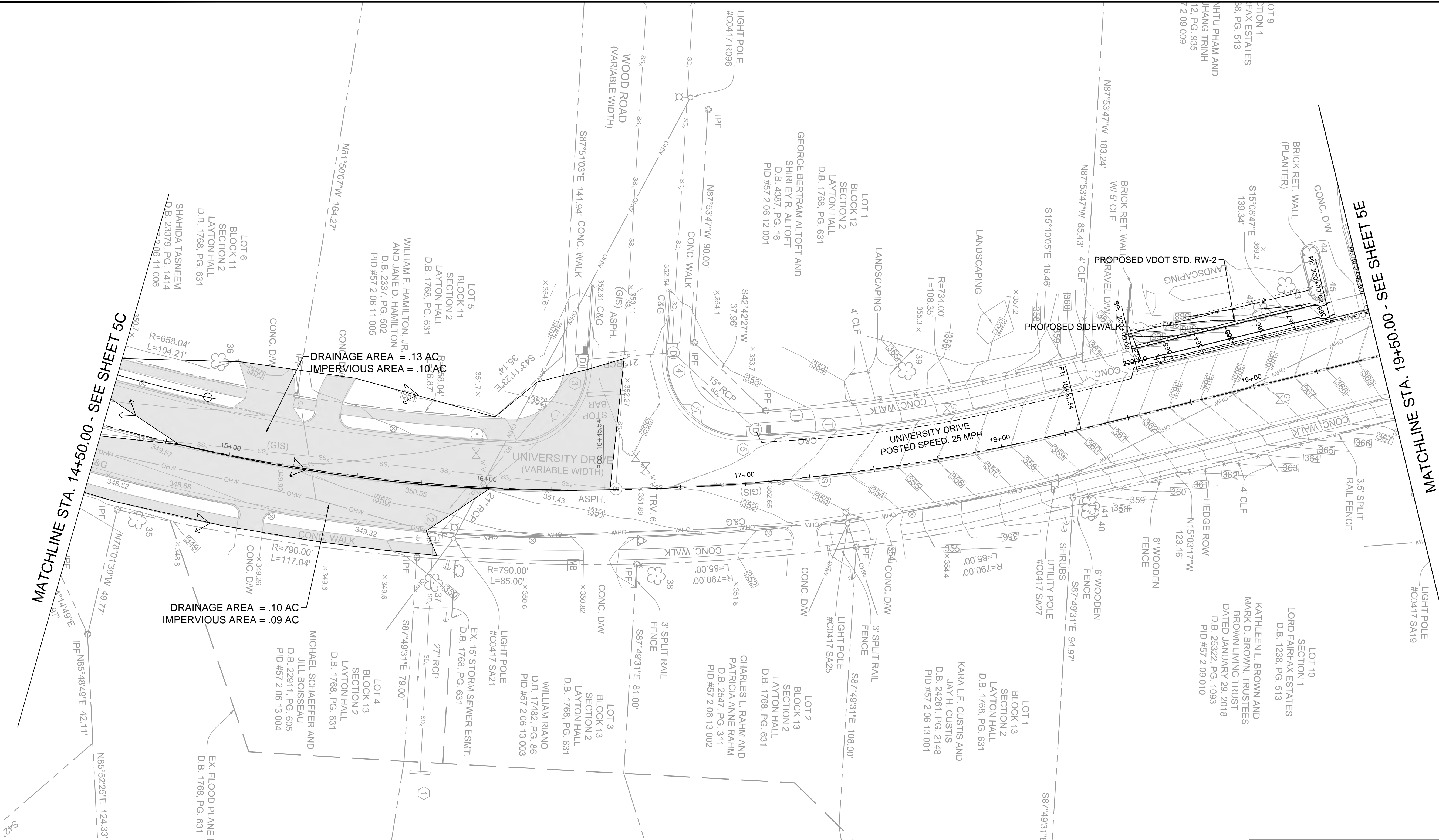
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
EROSION AND SEDIMENT CONTROL

UPC # 113121

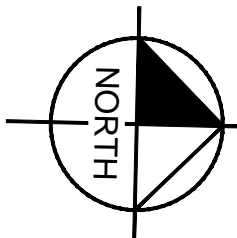
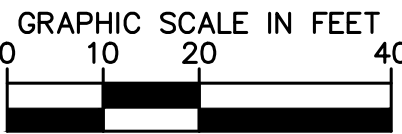
SCALE  
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SCALE

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5D



LEGEND

- PROPOSED DRAINAGE AREA
- PROPOSED IMPERVIOUS DRAINAGE AREA
- TEMPORARY SILT FENCE
- INLET PROTECTION
- PROPOSED TEMPORARY EASEMENT
- PROPOSED LIMITS OF DISTURBANCE











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KENMORE DRIVE TO STRATFORD AVENUE  
EROSION AND SEDIMENT CONTROL

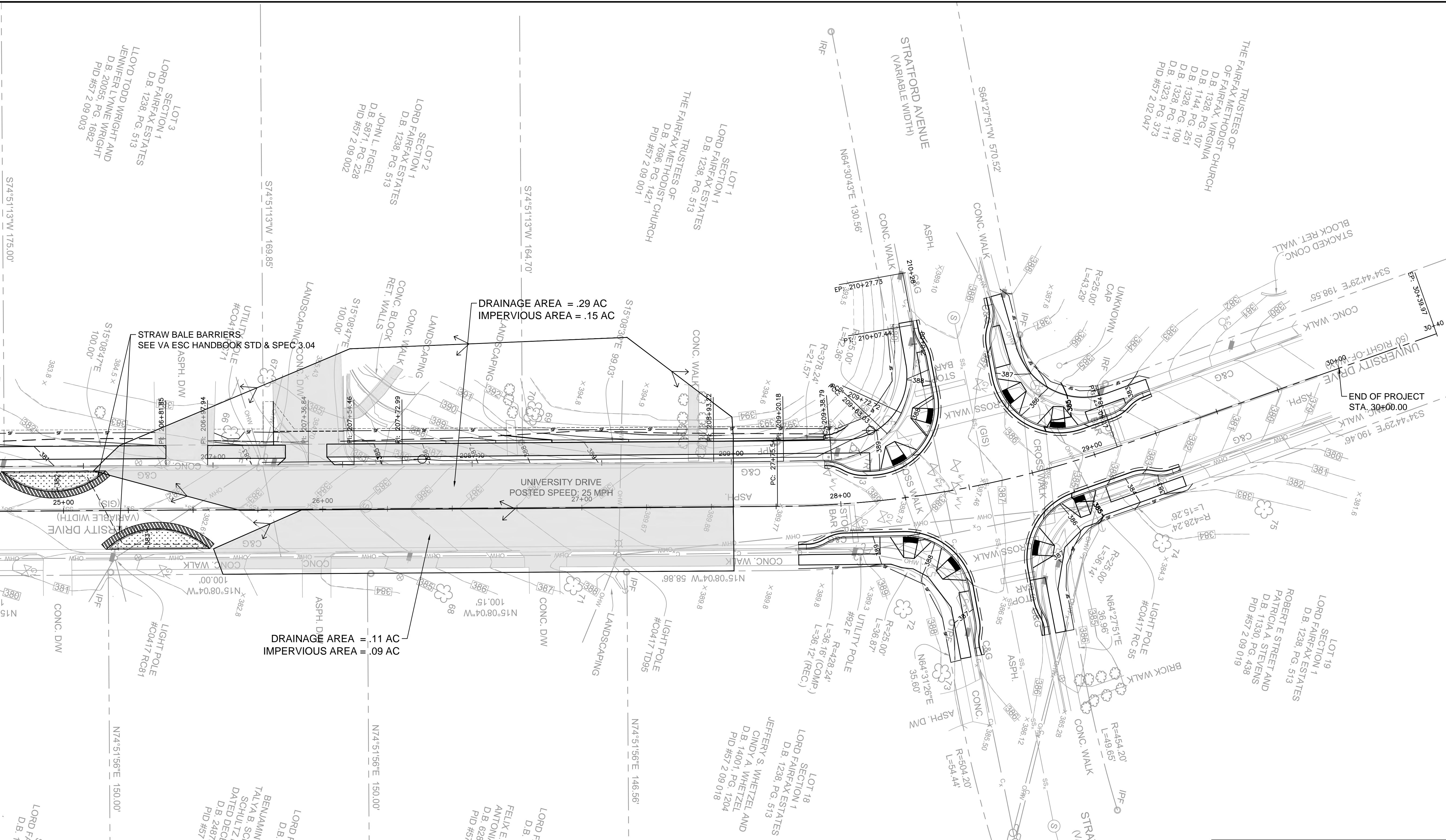
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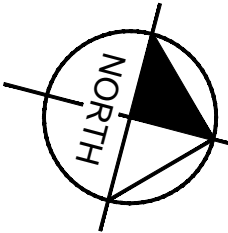
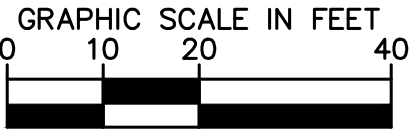
SHEET

5F

MATCHLINE STA. 24+75.00 - SEE SHEET 4E



- LEGEND**
- PROPOSED DRAINAGE AREA
  - PROPOSED IMPERVIOUS DRAINAGE AREA
  - TEMPORARY SILT FENCE
  - INLET PROTECTION
  - PROPOSED TEMPORARY EASEMENT
  - PROPOSED LIMITS OF DISTURBANCE







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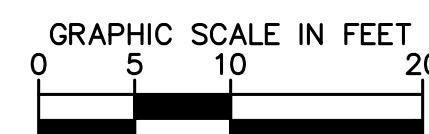
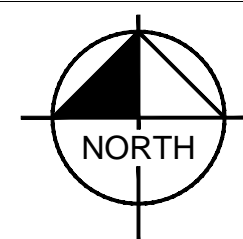
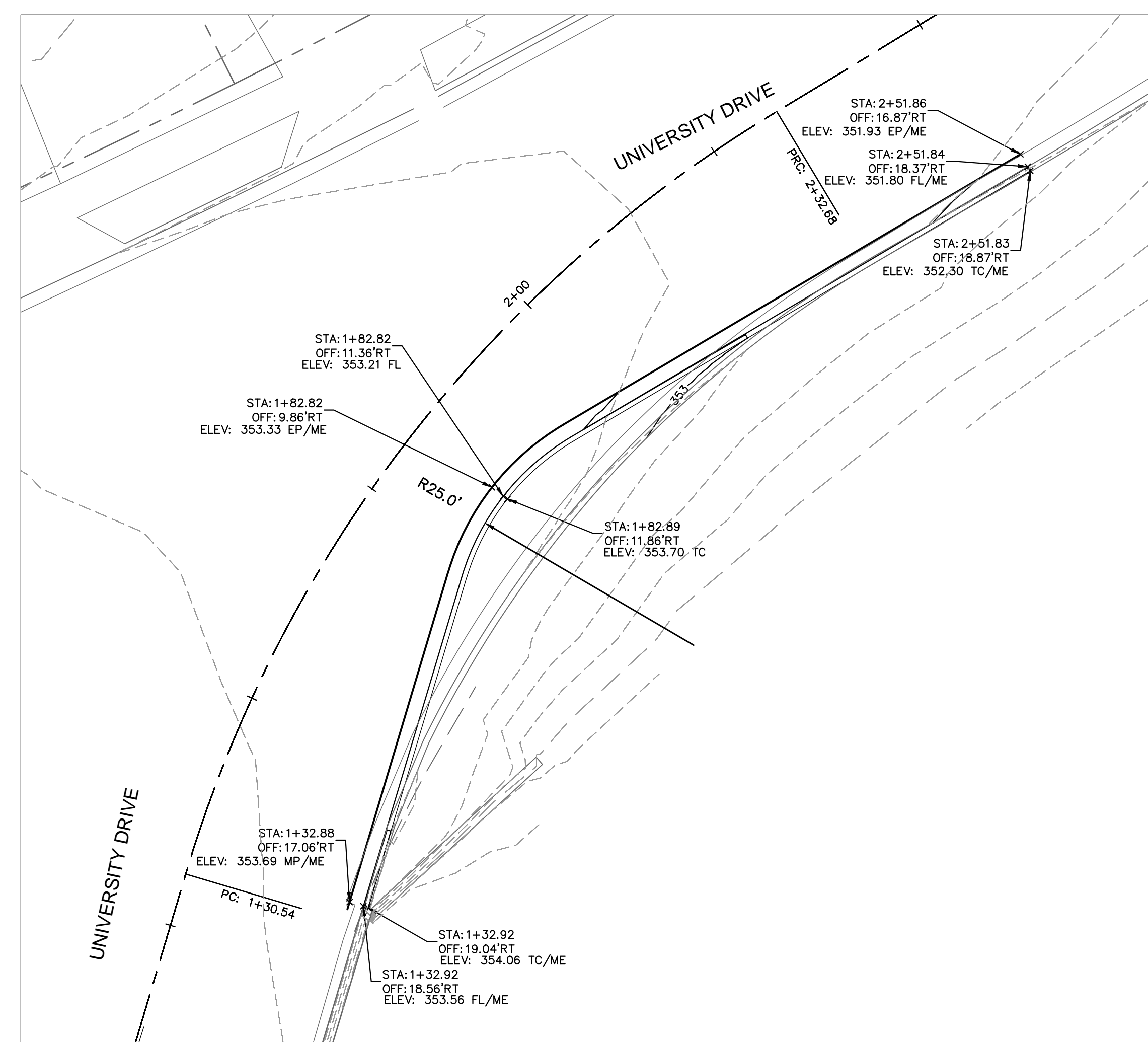
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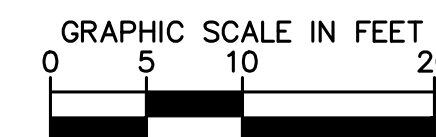
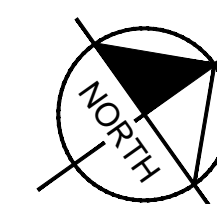
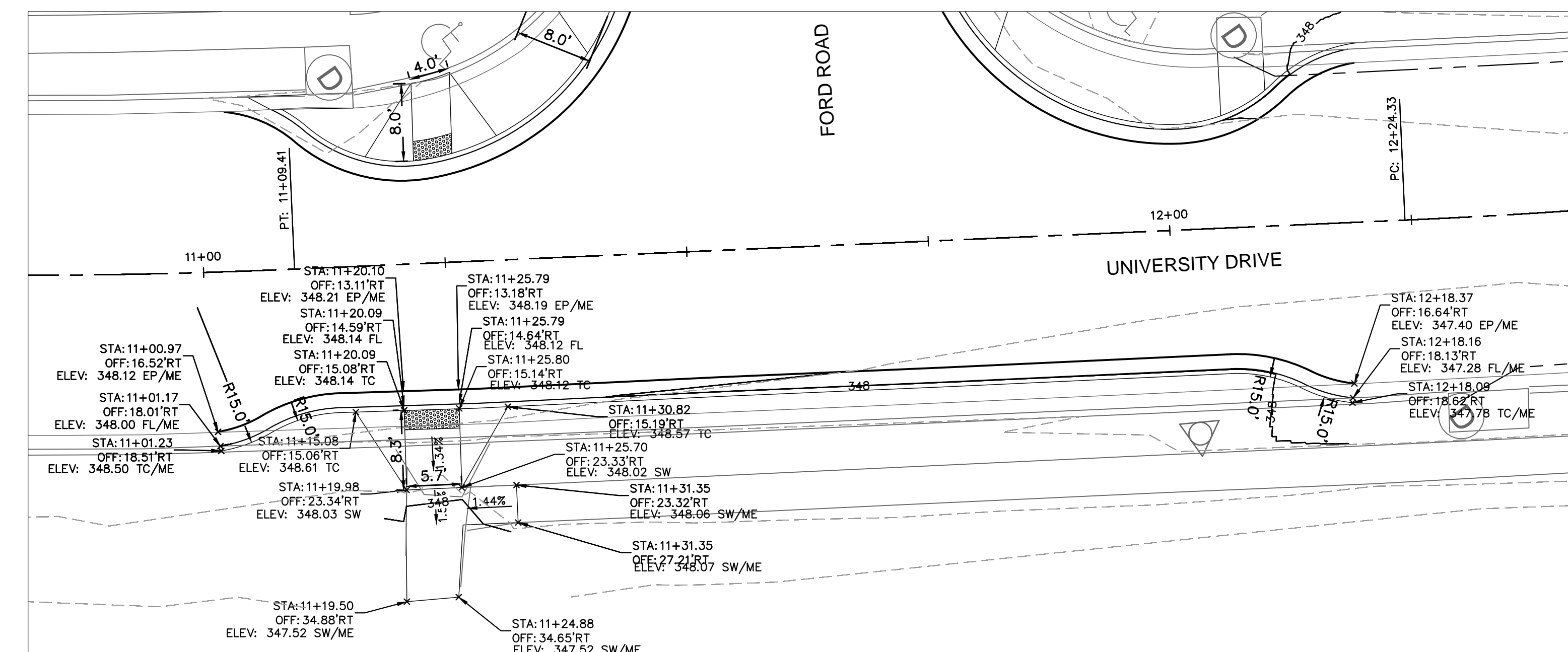
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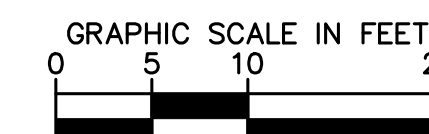
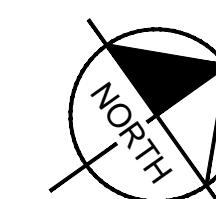
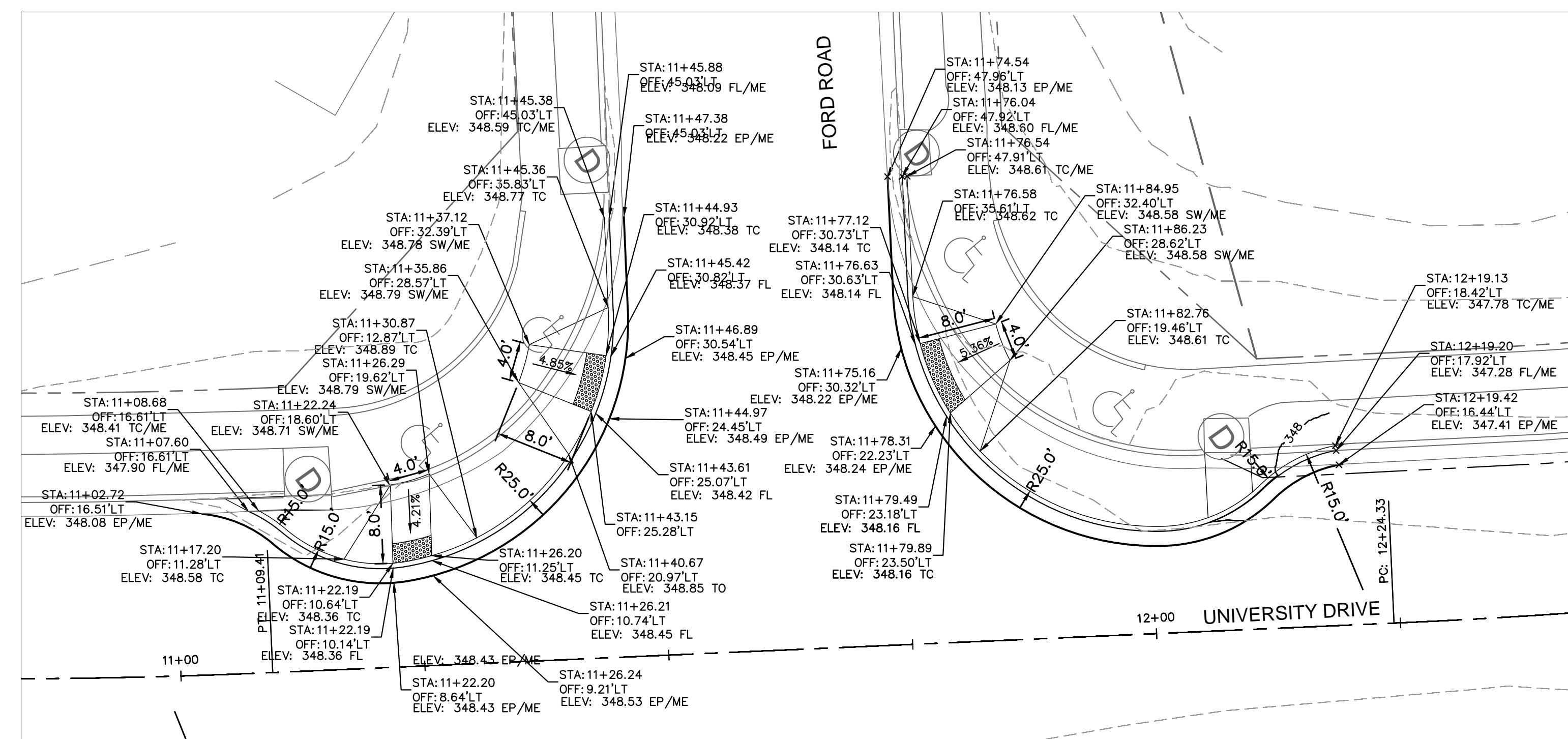
## CURB REALIGNMENT - UNIVERSITY DRIVE AND KENMORE DRIVE



## CURB EXTENSIONS - FORD ROAD



## CURB EXTENSIONS - FORD ROAD



LEGEND

SW - SIDEWALK  
TC - TOP OF CURB  
ME - MATCH EXISTING ELEVATION  
FL - FLOW LINE  
EP - EDGE OF PAVEMENT

## NOTES

1. GRADE AROUND PROPOSED ASPHALT AND CURB RAMPS TO ENSURE POSITIVE DRAINAGE.
2. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.

CITY OF FAIRFAX, VIRGINIA  
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# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

### CURB CUT RAMP DETAILS

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET

6A



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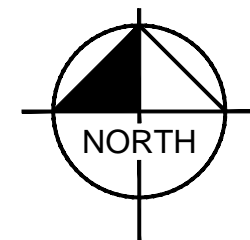
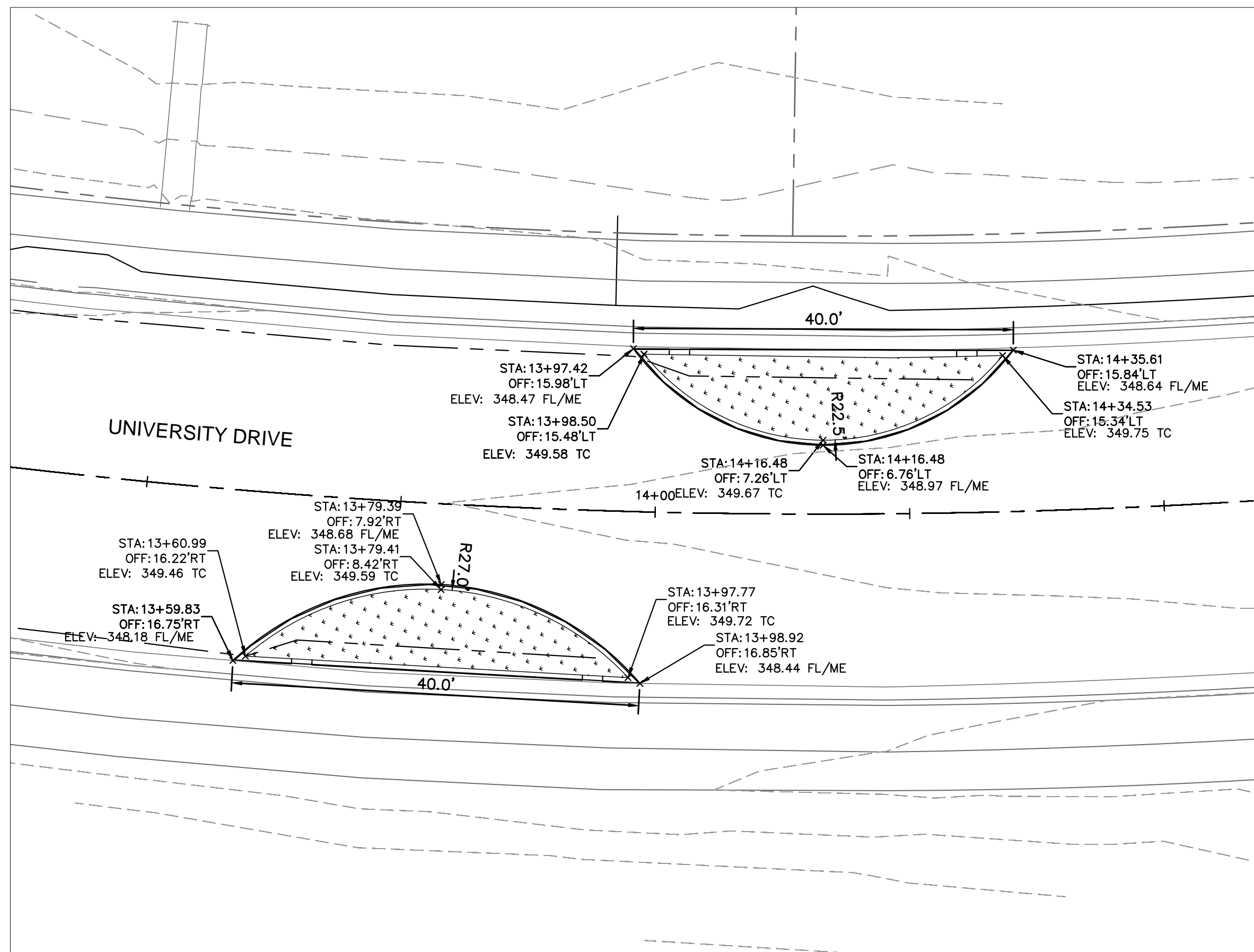
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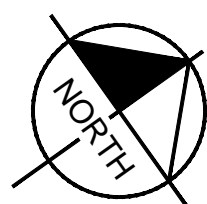
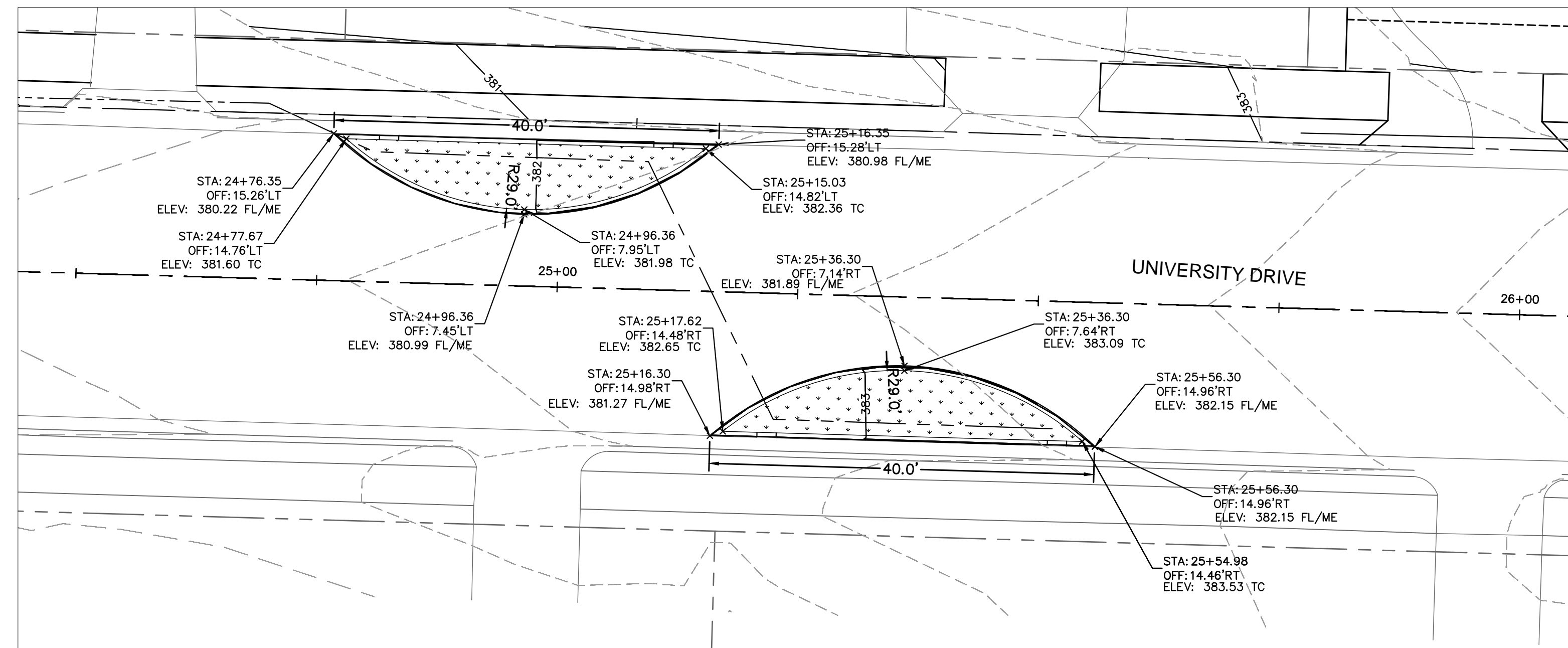
## CHICANES - NORTH OF FORD ROAD




GRAPHIC SCALE IN FEET



## CHICANES - NORTH OF JEAN STREET

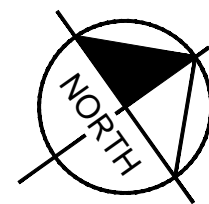
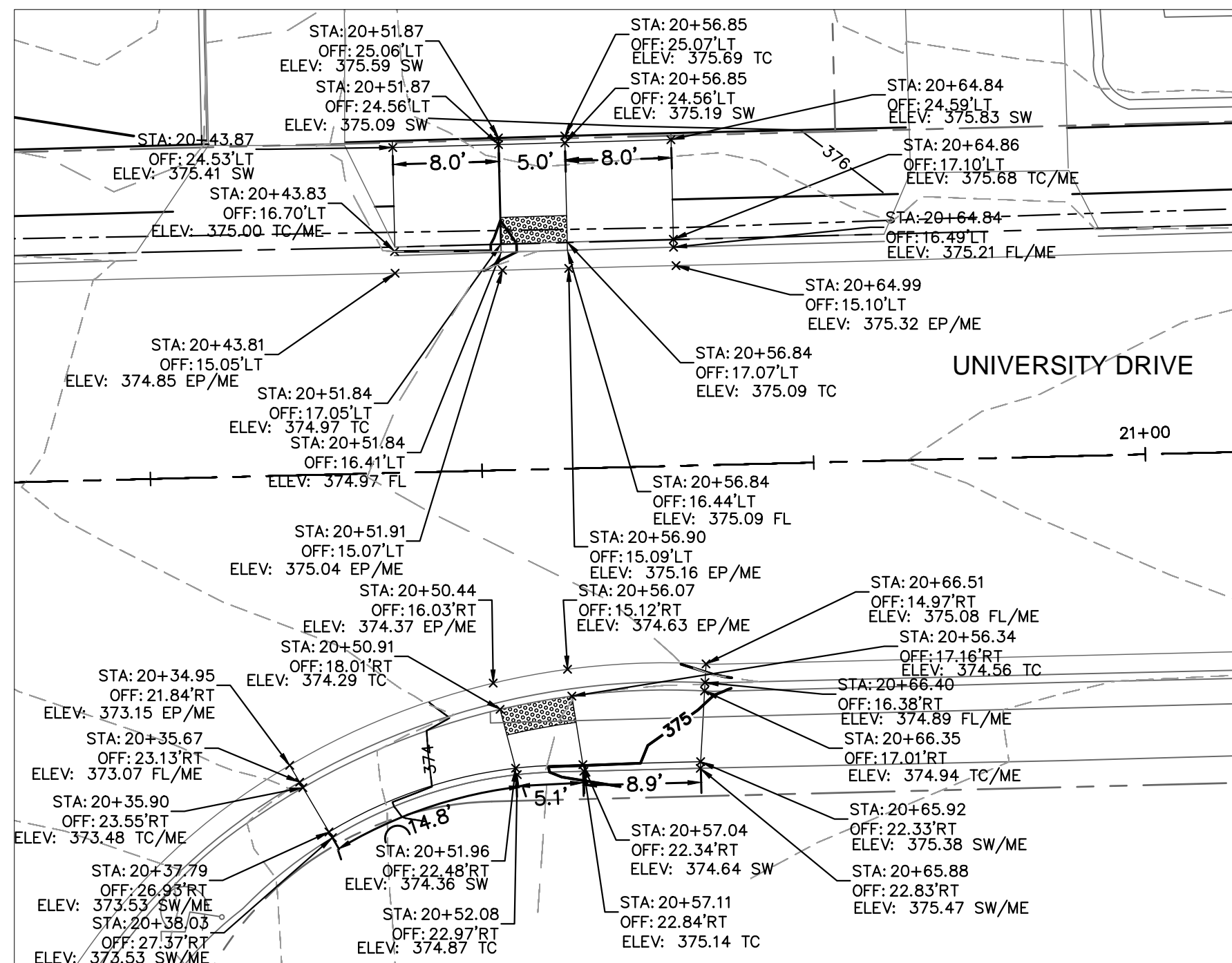


GRAPHIC SCALE IN FEET




A horizontal line with vertical tick marks at 0, 5, 10, and 20. The segment between 0 and 5 is shaded black. The segment between 5 and 10 is white. The segment between 10 and 20 is shaded black.

## CURB RAMPS - JEAN STREET



GRAPHIC SCALE IN FEET



LEGEND

SW - SIDEWALK  
TC - TOP OF CURB  
ME - MATCH EXISTING ELEVATION  
FL - FLOW LINE  
EP - EDGE OF PAVEMENT

## NOTES

1. GRADE AROUND PROPOSED ASPHALT AND CURB RAMPS TO ENSURE POSITIVE DRAINAGE.
2. DO NOT DISTURB EXISTING STORM DRAIN INLETS UNLESS OTHERWISE NOTED ON PLAN.

CITY OF FAIRFAX, VIRGINIA  
DEPARTMENT OF PUBLIC WORKS

# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

### CURB CUT RAMP DETAILS

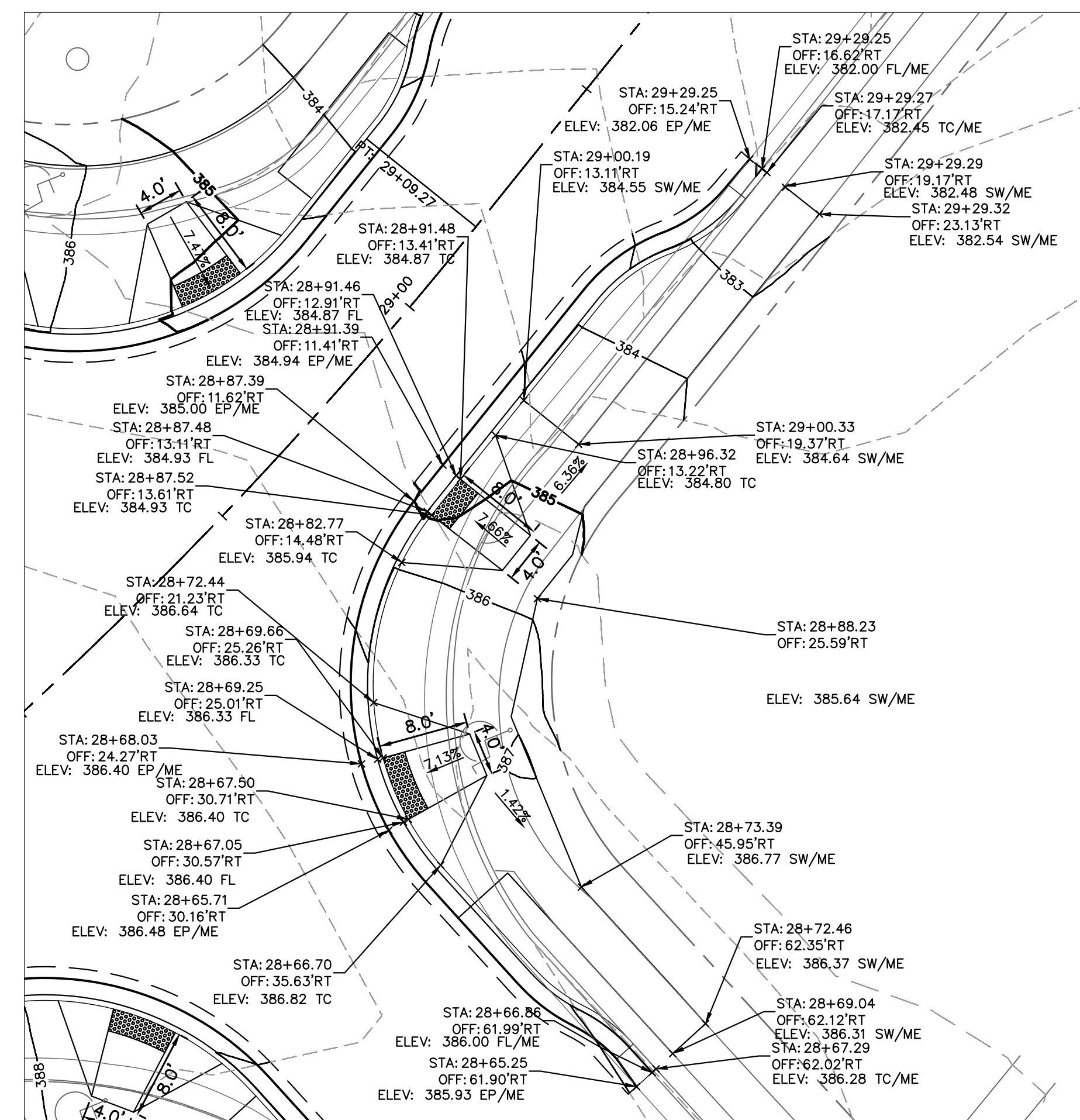
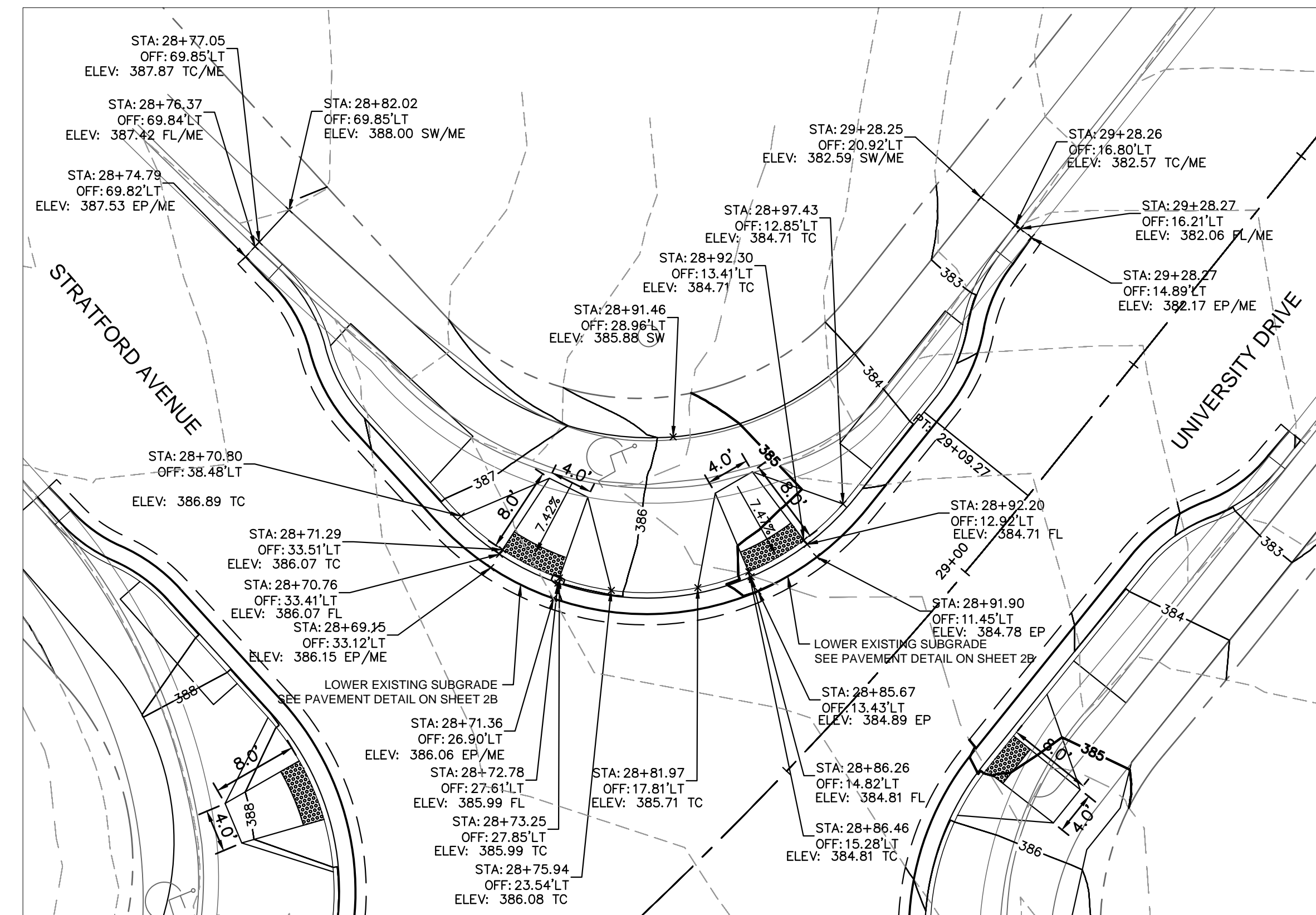
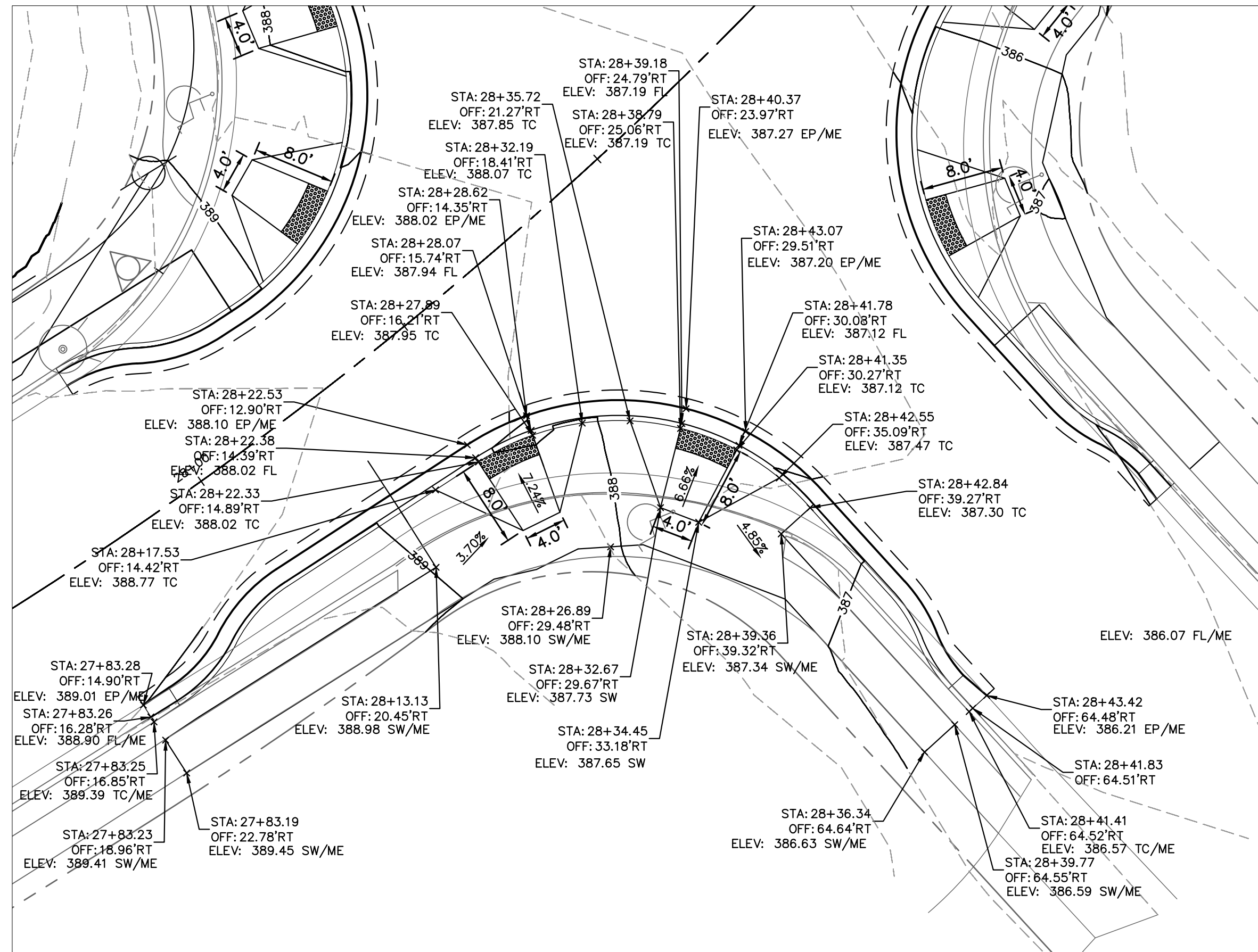
UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

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6B





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DEPARTMENT OF PUBLIC WORKS

# UNIVERSITY DRIVE TRAFFIC CALMING

## KENMORE DRIVE TO STRATFORD AVENUE

### CURB CUT RAMP DETAILS

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE



CITY OF FAIRFAX

## DEPARTMENT OF PUBLIC WORKS

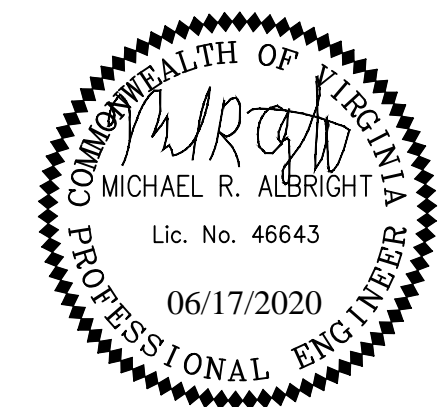
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Phone: 703-385-7889

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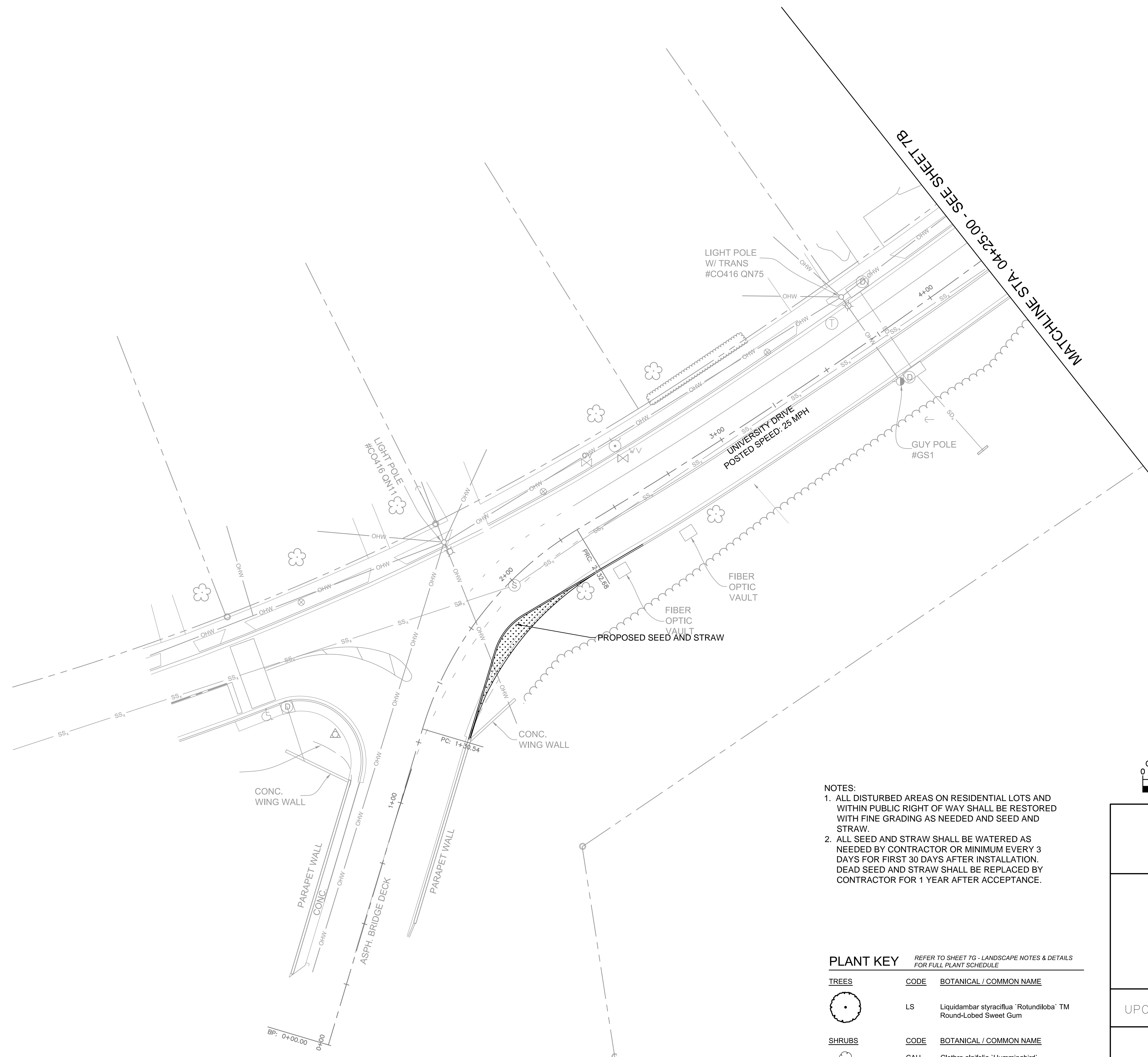
UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET

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NOTES:

1. ALL DISTURBED AREAS ON RESIDENTIAL LOTS AND WITHIN PUBLIC RIGHT OF WAY SHALL BE RESTORED WITH FINE GRADING AS NEEDED AND SEED AND STRAW.
2. ALL SEED AND STRAW SHALL BE WATERED AS NEEDED BY CONTRACTOR OR MINIMUM EVERY 3 DAYS FOR FIRST 30 DAYS AFTER INSTALLATION. DEAD SEED AND STRAW SHALL BE REPLACED BY CONTRACTOR FOR 1 YEAR AFTER ACCEPTANCE.

## PLANT KEY

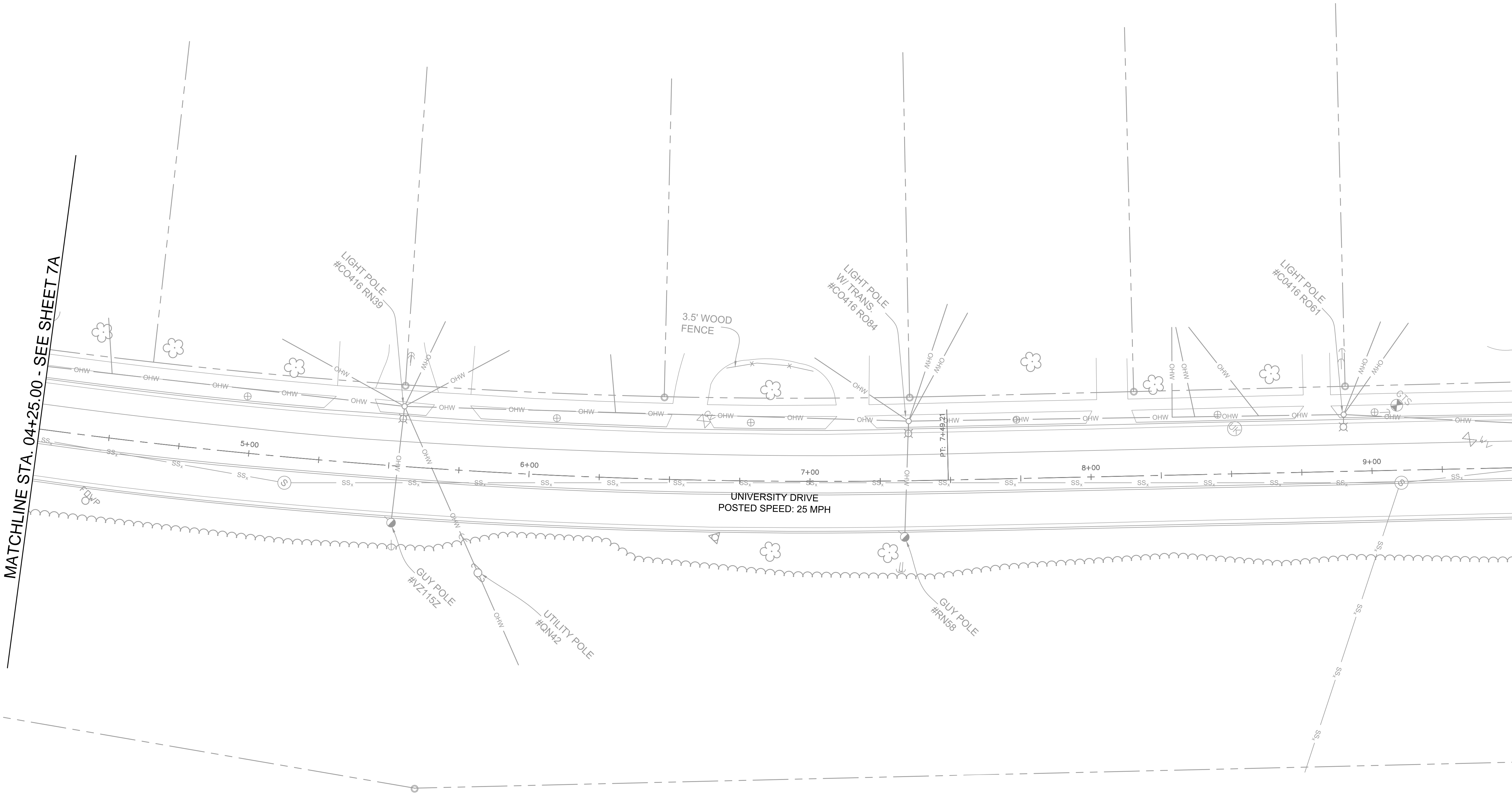
REFER TO SHEET 7G - LANDSCAPE NOTES & DETAILS  
FOR FULL PLANT SCHEDULE

| TREES   |     | CODE | BOTANICAL / COMMON NAME   |
|---|-----|------|---|
|  | LS  |      | Liquidambar styraciflua 'Rotundiloba' TM<br>Round-Lobed Sweet Gum |
| SHRUBS  |     | CODE | BOTANICAL / COMMON NAME   |
|  | CAH |      | Clethra alnifolia 'Hummingbird'<br>Summersweet Clethra            |
| GROUND COVERS   |     | CODE | BOTANICAL / COMMON NAME   |
|  | SSC |      | Schizachyrium scoparium<br>Little Bluestem Grass                  |



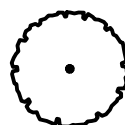

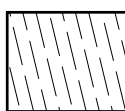
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MATCHLINE STA. 04+25.00 - SEE SHEET 7A



MATCHLINE STA. 09+50.00 - SEE SHEET 7C

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| PLANT KEY   |      |   | REFER TO SHEET 7G - LANDSCAPE NOTES & DETAILS FOR FULL PLANT SCHEDULE |
|---|------|---|---|
| TREES   | CODE | BOTANICAL / COMMON NAME   |   |
|  | LS   | Liquidambar styraciflua 'Rotundiloba' TM<br>Round-Lobed Sweet Gum |   |
| SHRUBS  | CODE | BOTANICAL / COMMON NAME   |   |
|  | CAH  | Clethra alnifolia 'Hummingbird'<br>Summersweet Clethra            |   |
| GROUND COVERS   | CODE | BOTANICAL / COMMON NAME   |   |
|  | SSC  | Schizachyrium scoparium<br>Little Bluestem Grass                  |   |



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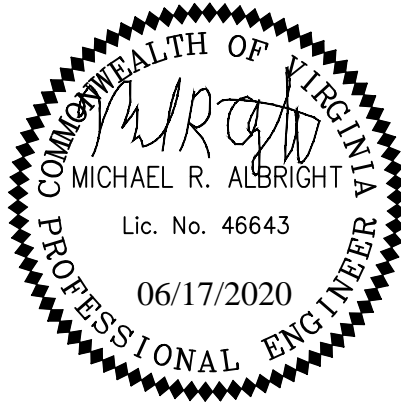
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

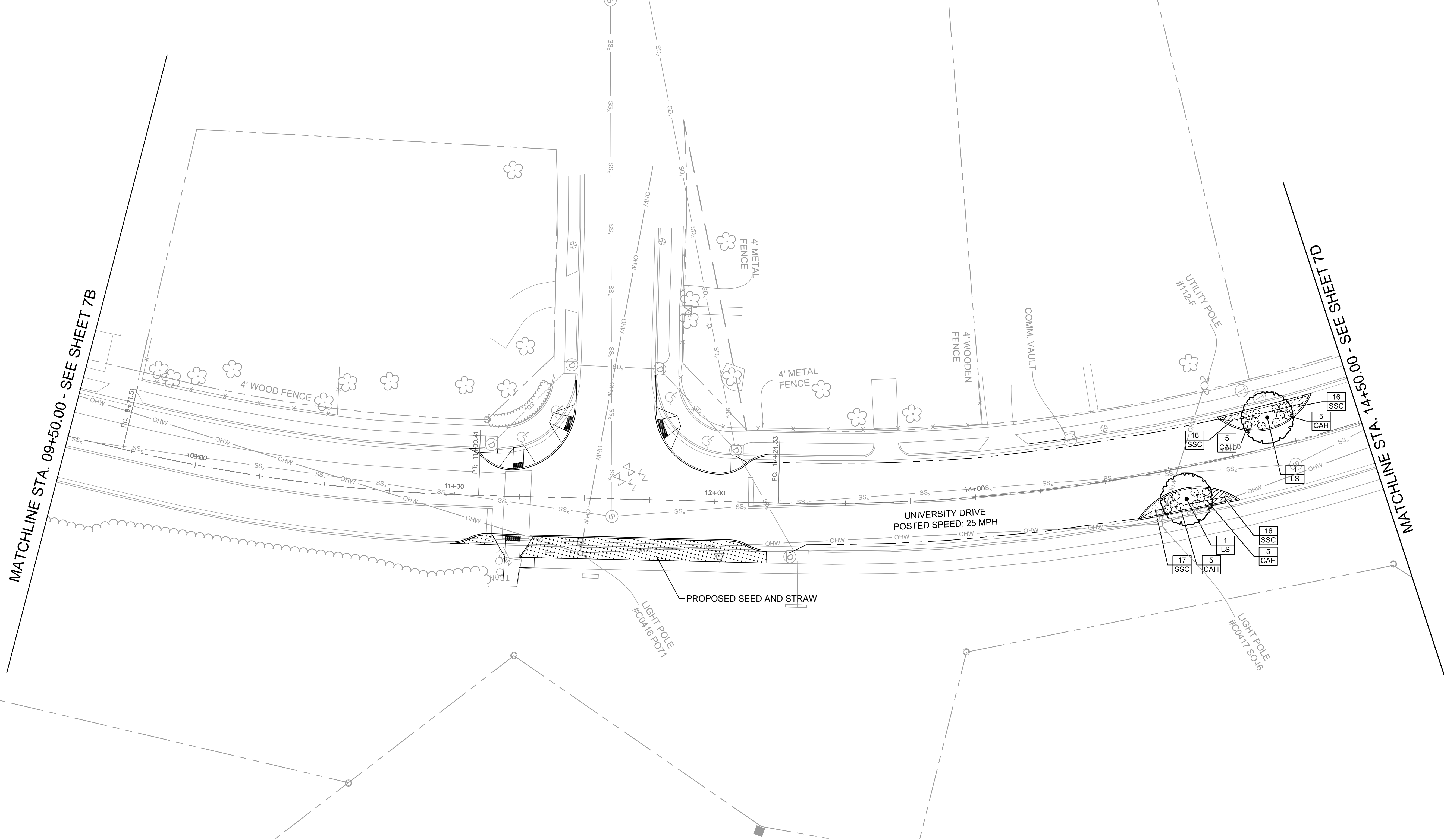
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MATCHLINE STA. 09+50.00 - SEE SHEET 7B



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|---------------|------|--|---|
| TREES         | CODE | BOTANICAL / COMMON NAME  |   |
|               | LS   | Liquidambar styraciflua 'Rotundiloba' TM Round-Lobed Sweet Gum |   |
| SHRUBS        | CODE | BOTANICAL / COMMON NAME  |   |
|               | CAH  | Clethra alnifolia 'Hummingbird' Summersweet Clethra            |   |
| GROUND COVERS | CODE | BOTANICAL / COMMON NAME  |   |
|               | SSC  | Schizachyrium scoparium Little Bluestem Grass                  |   |



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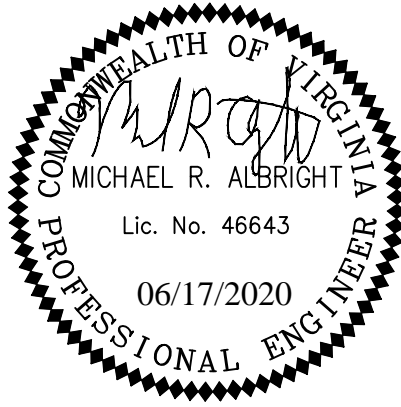
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

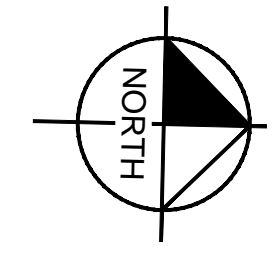
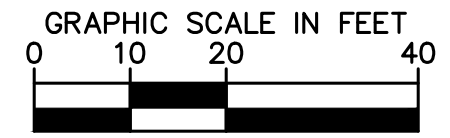
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MATCHLINE STA. 14+50.00 - SEE SHEET 7C



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| PLANT KEY     |      |   | REFER TO SHEET 7G - LANDSCAPE NOTES & DETAILS FOR FULL PLANT SCHEDULE |
|---------------|------|---|---|
| TREES         | CODE | BOTANICAL / COMMON NAME   |   |
|               | LS   | Liquidambar styraciflua 'Rotundiloba' TM<br>Round-Lobed Sweet Gum |   |
| SHRUBS        | CODE | BOTANICAL / COMMON NAME   |   |
|               | CAH  | Clethra alnifolia 'Hummingbird'<br>Summersweet Clethra            |   |
| GROUND COVERS | CODE | BOTANICAL / COMMON NAME   |   |
|               | SSC  | Schizachyrium scoparium<br>Little Bluestem Grass                  |   |

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET  
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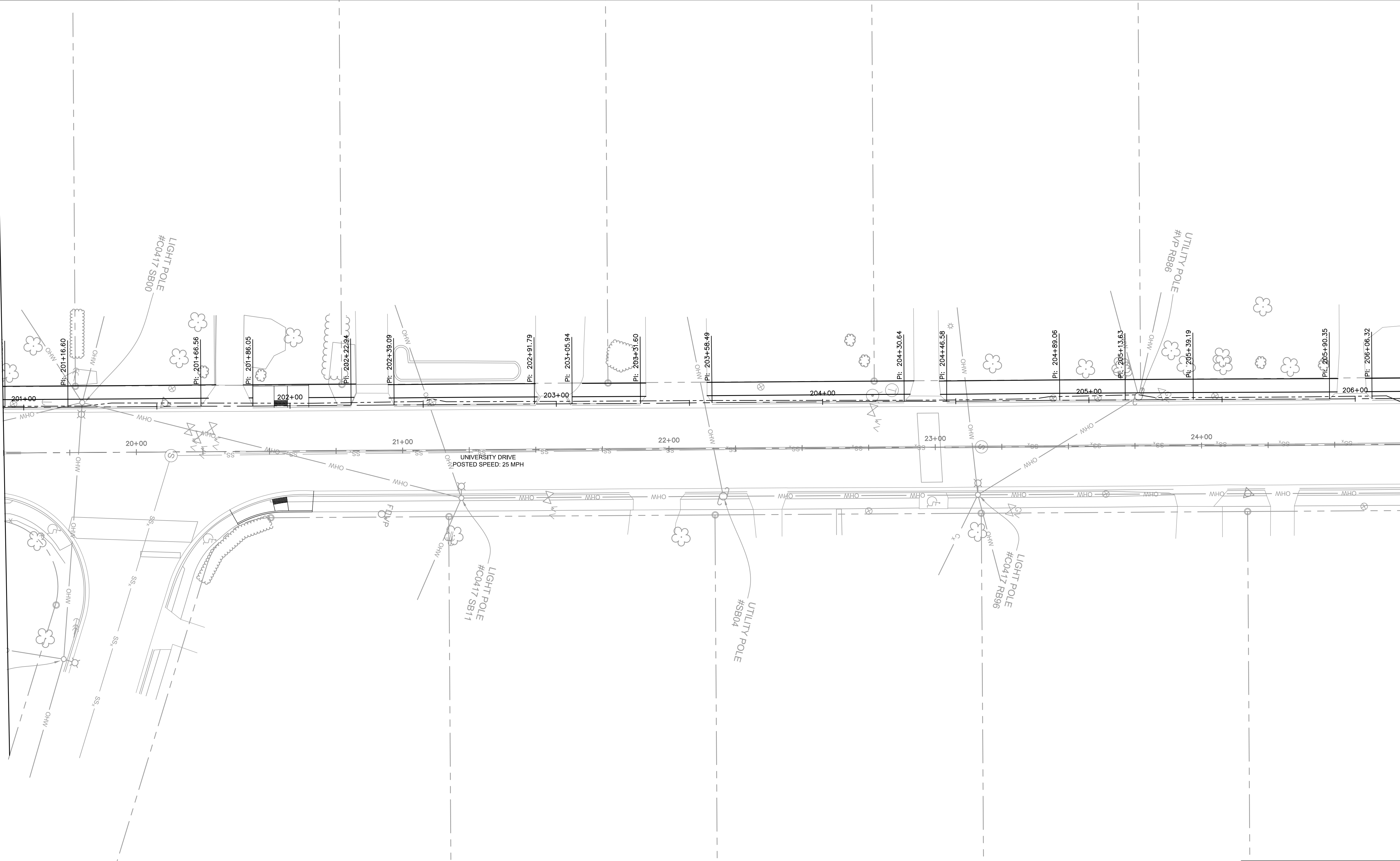
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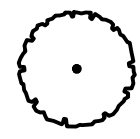

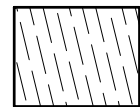
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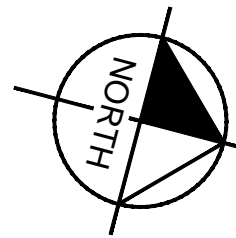
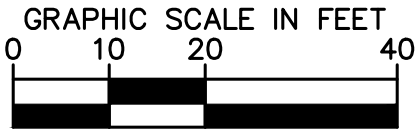
MATCHLINE STA. 19+50.00 - SEE SHEET 7D



MATCHLINE STA. 24+75.00 - SEE SHEET 7F

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

SCALE  
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CITY OF FAIRFAX

DEPARTMENT OF PUBLIC WORKS

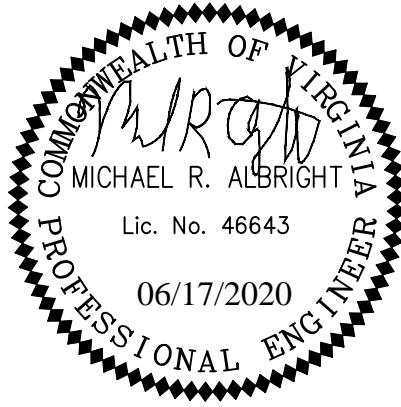
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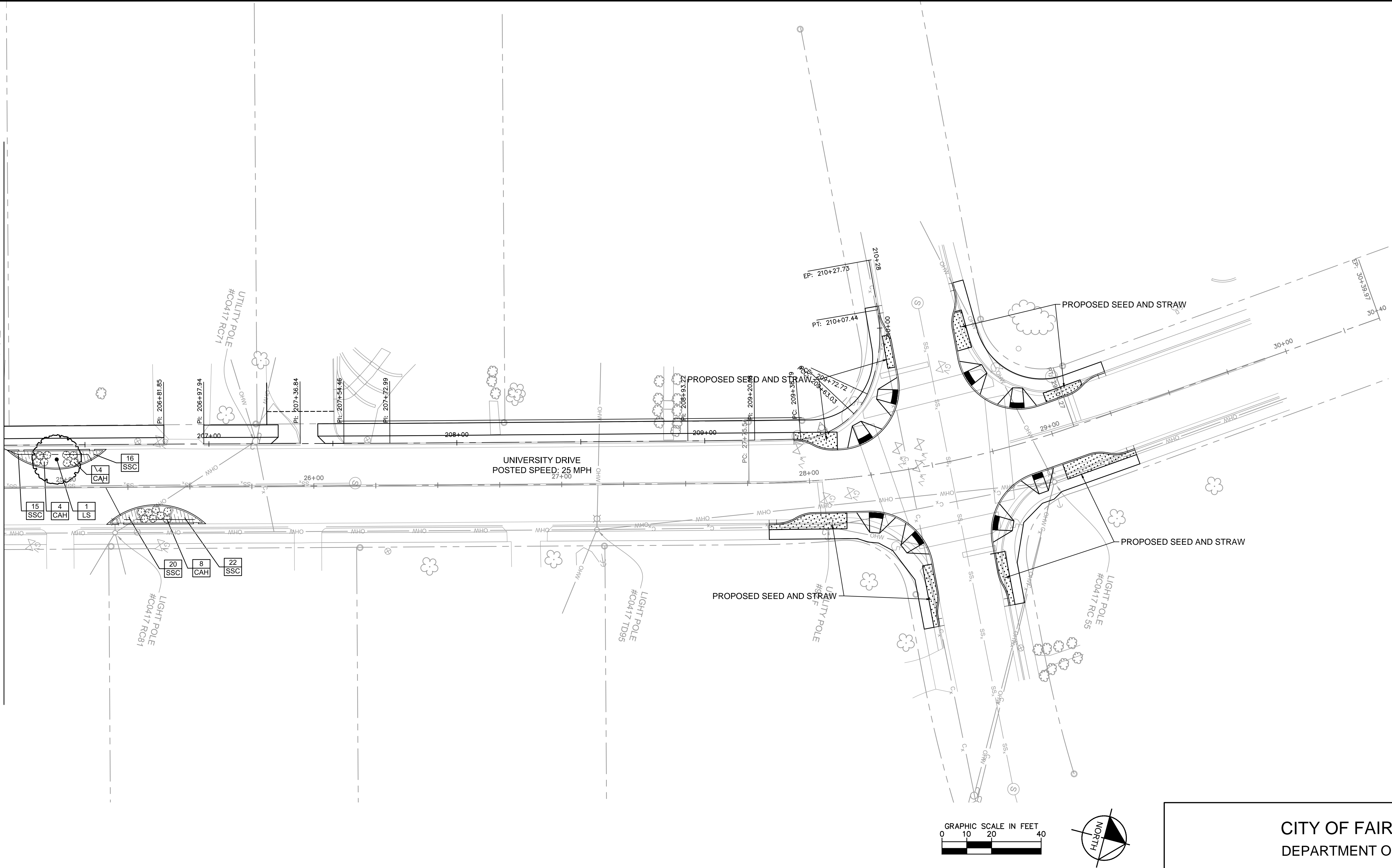
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| SHRUBS        | CODE | BOTANICAL / COMMON NAME  |   |
|               | CAH  | Clethra alnifolia 'Hummingbird' Summersweet Clethra            |   |
| GROUND COVERS | CODE | BOTANICAL / COMMON NAME  |   |
|               | SSC  | Schizachyrium scoparium Little Bluestem Grass                  |   |

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
LANDSCAPE PLAN

UPC # 113121

SCALE  
SEE GRAPHIC  
SCALE

SHEET  
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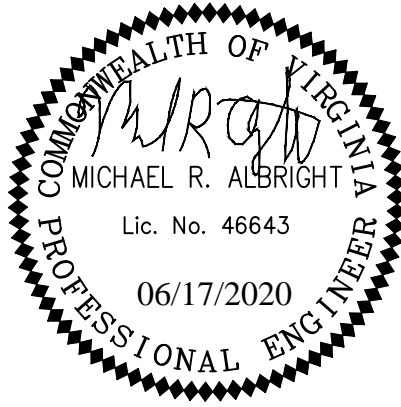
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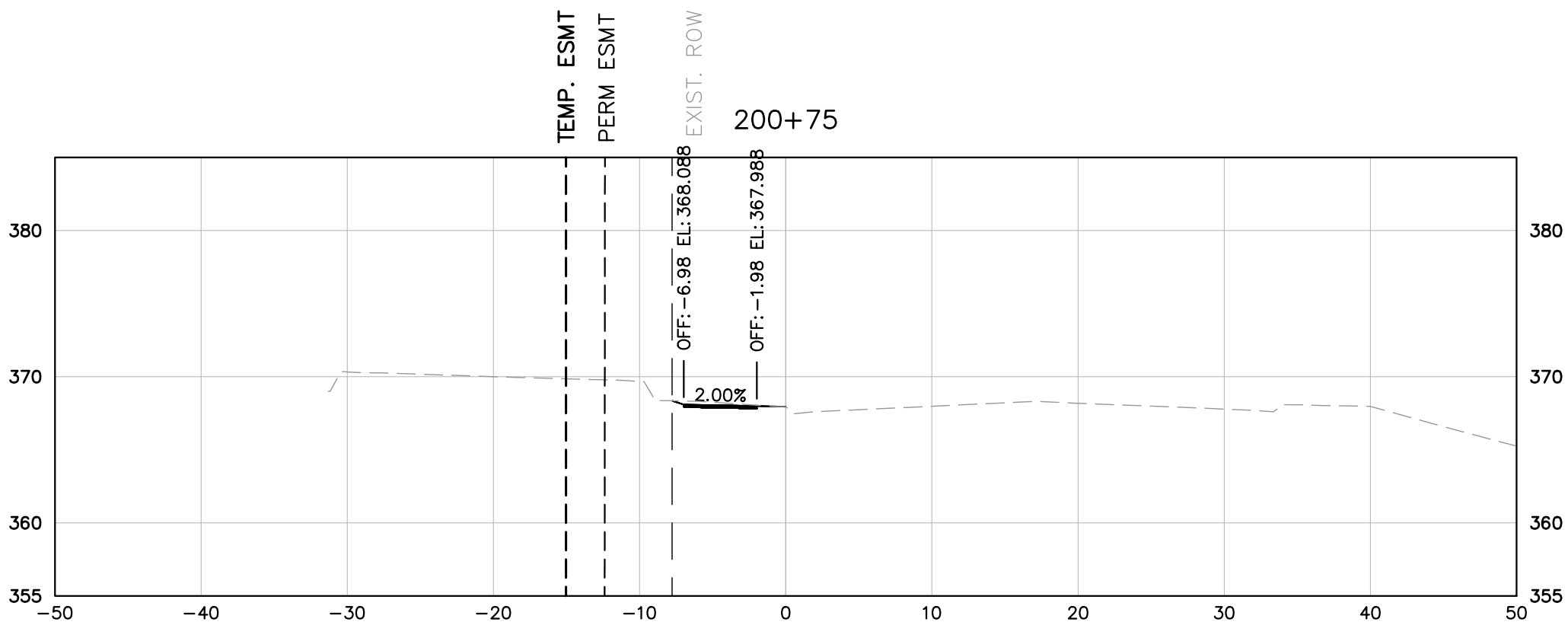
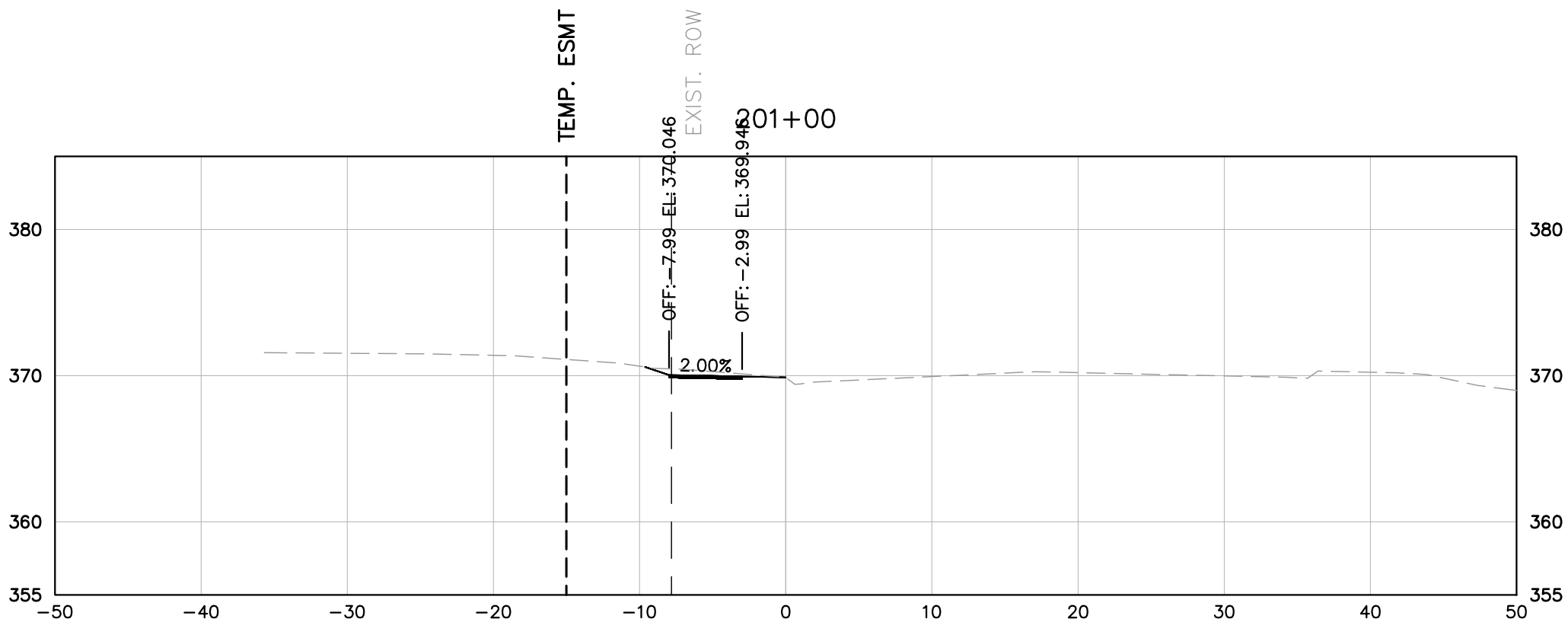
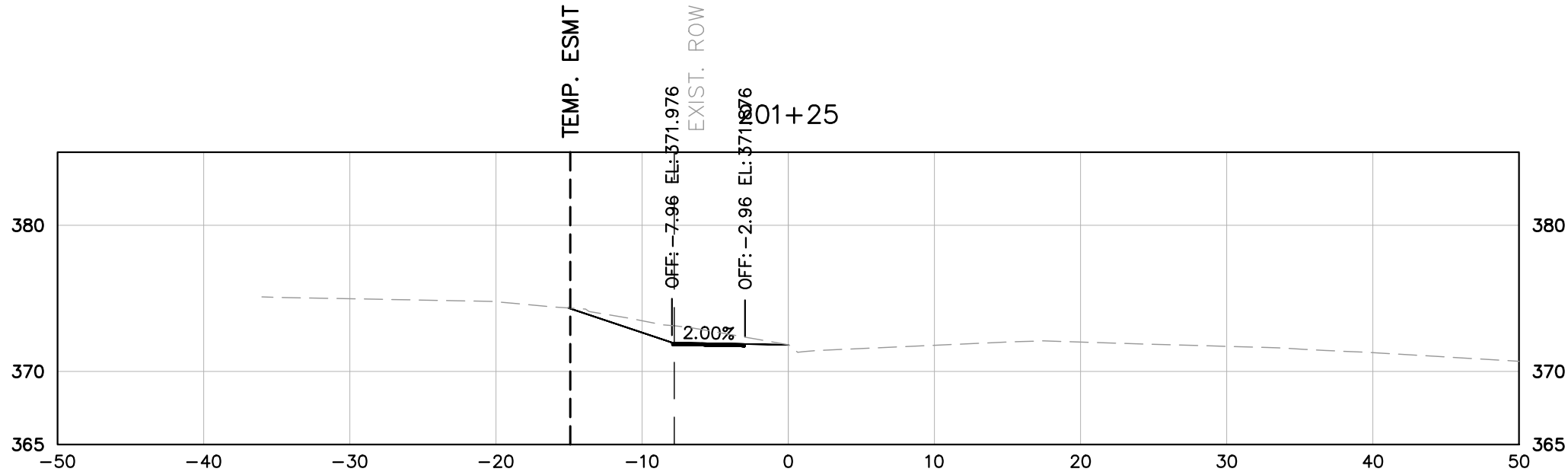
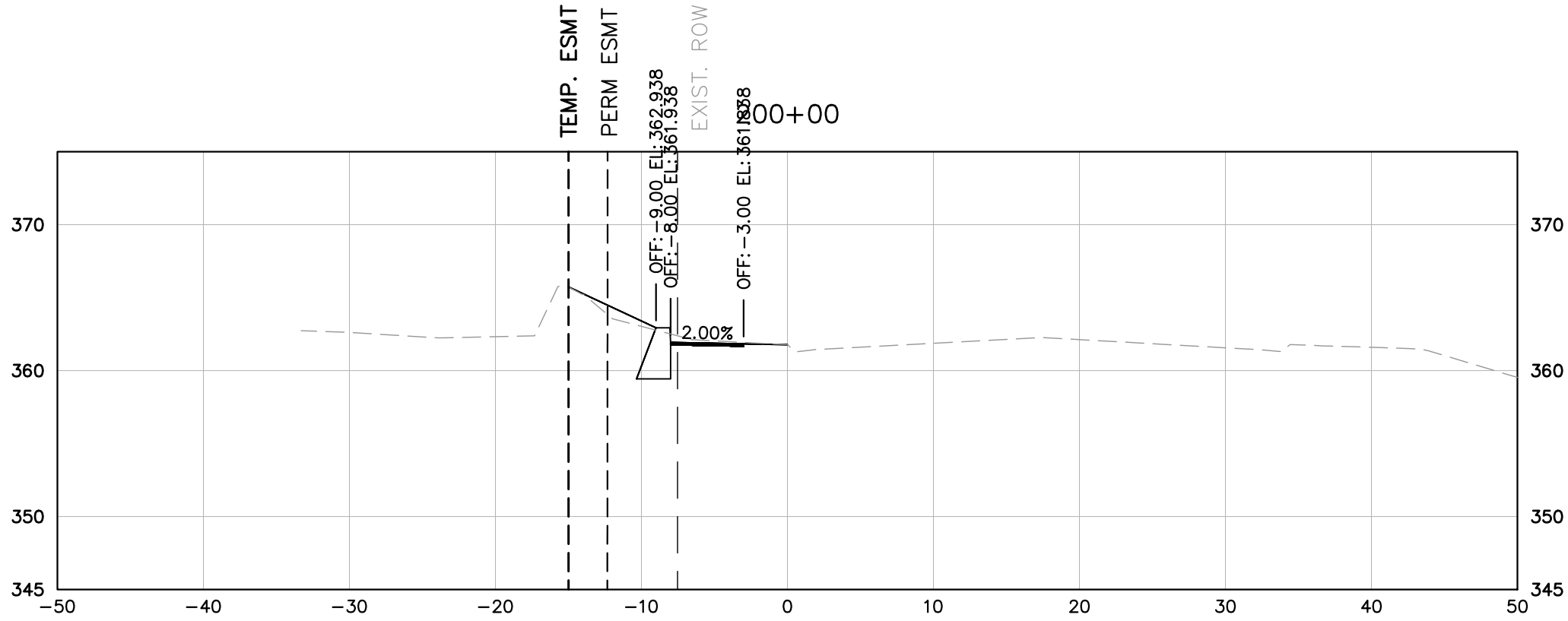
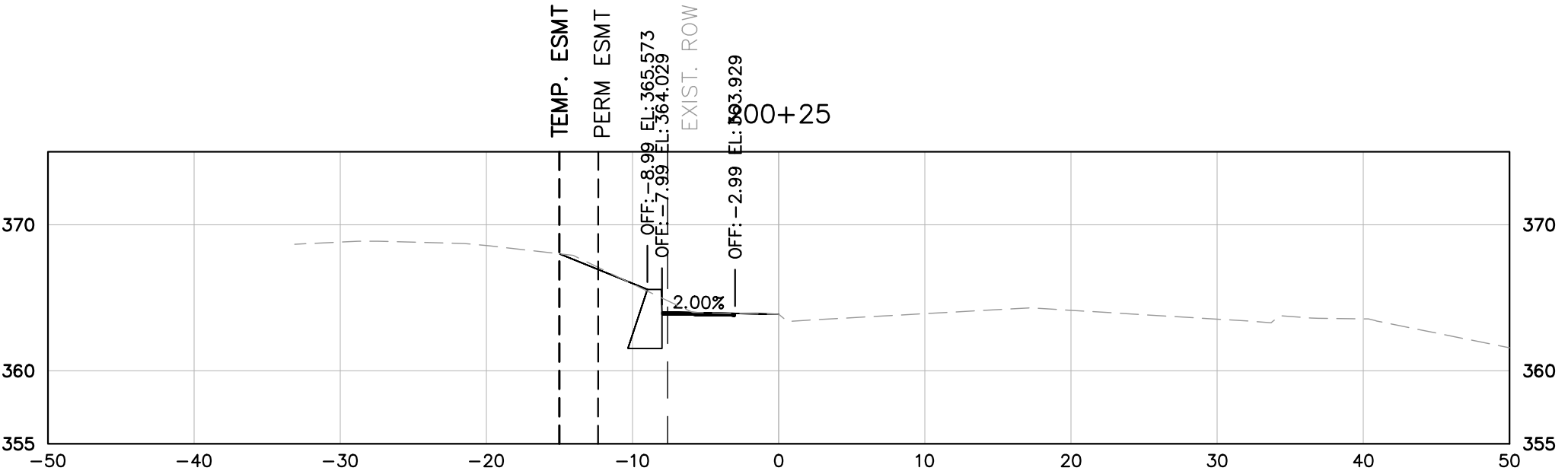
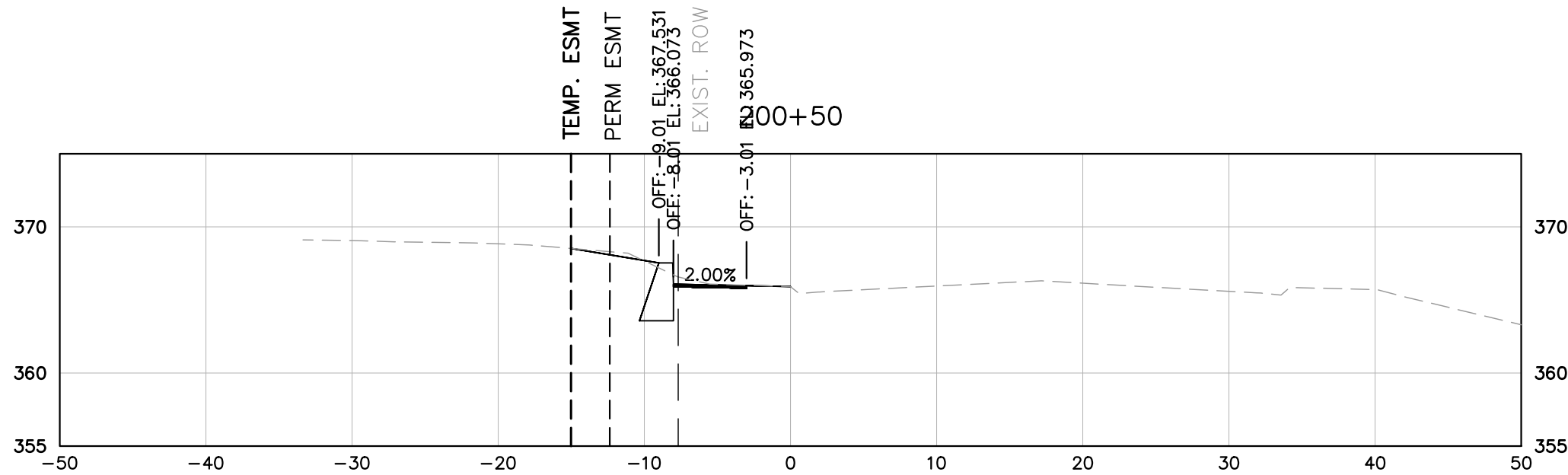
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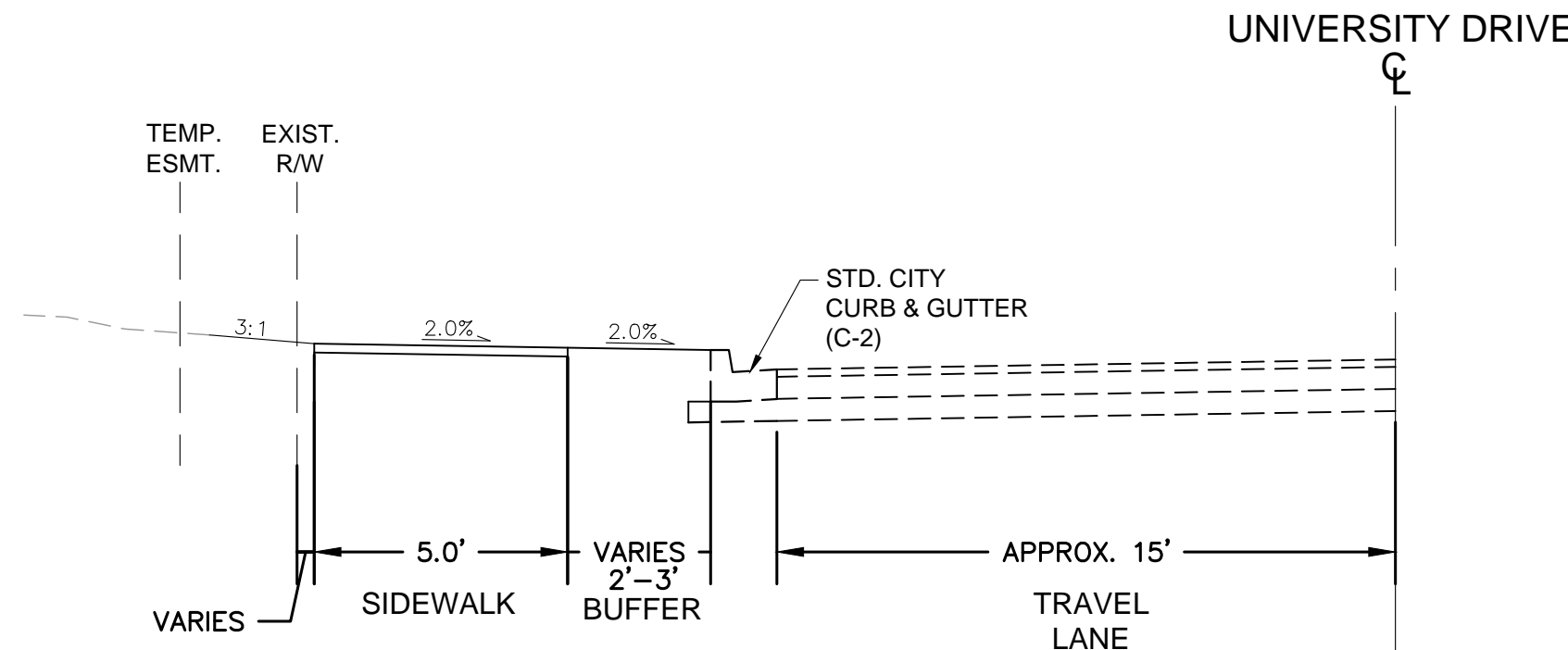
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TYPICAL SECTION

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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CROSS SECTIONS - SIDEWALK

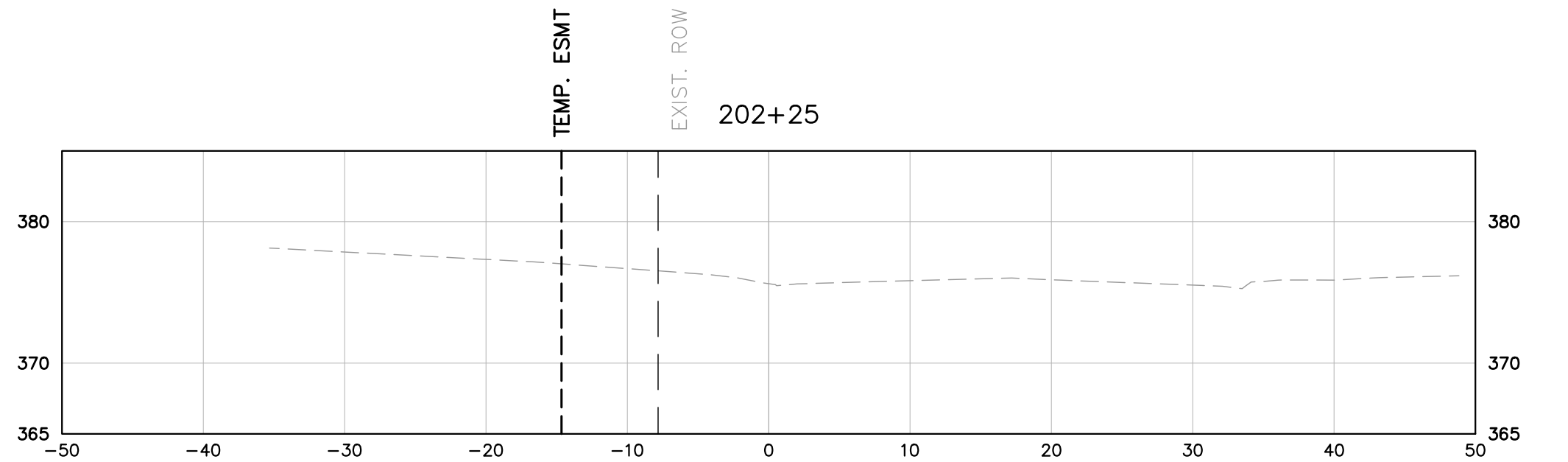
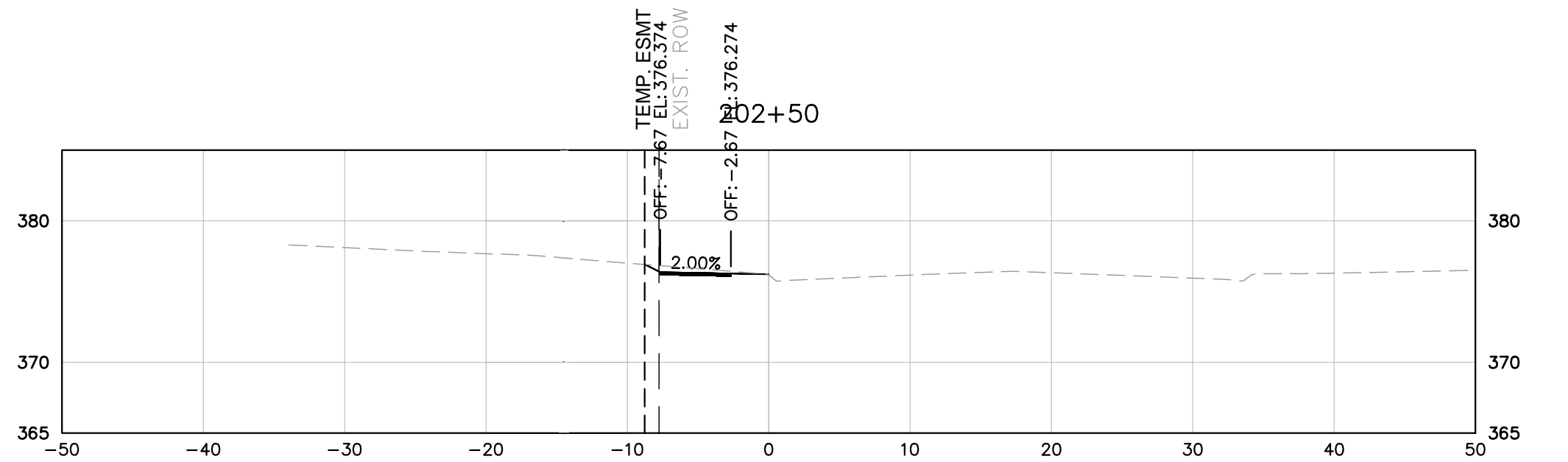
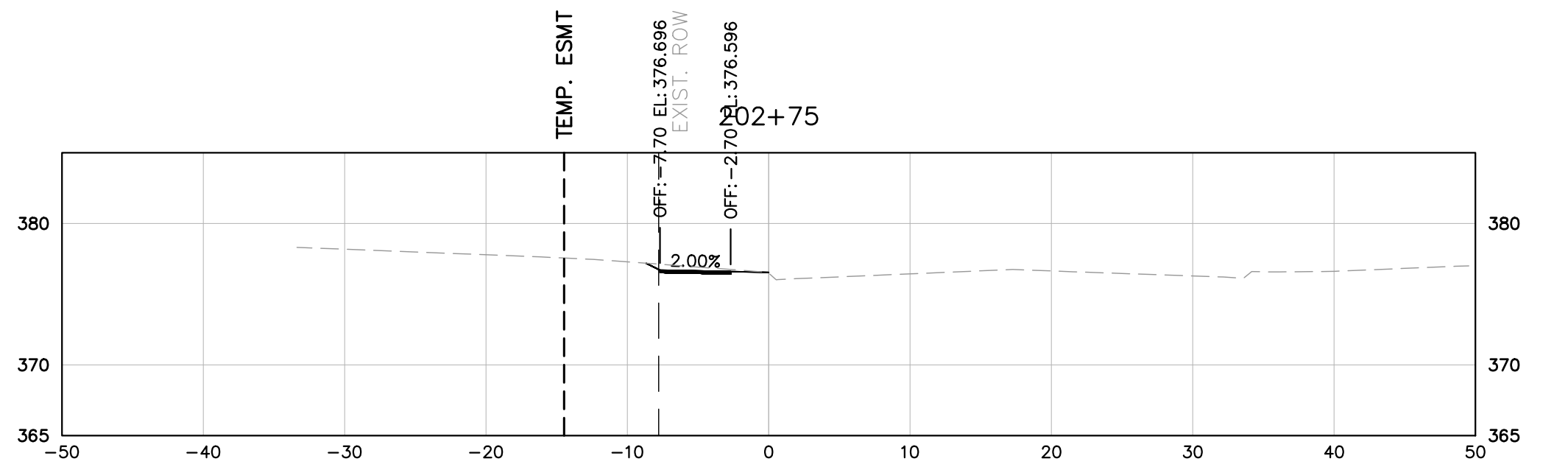
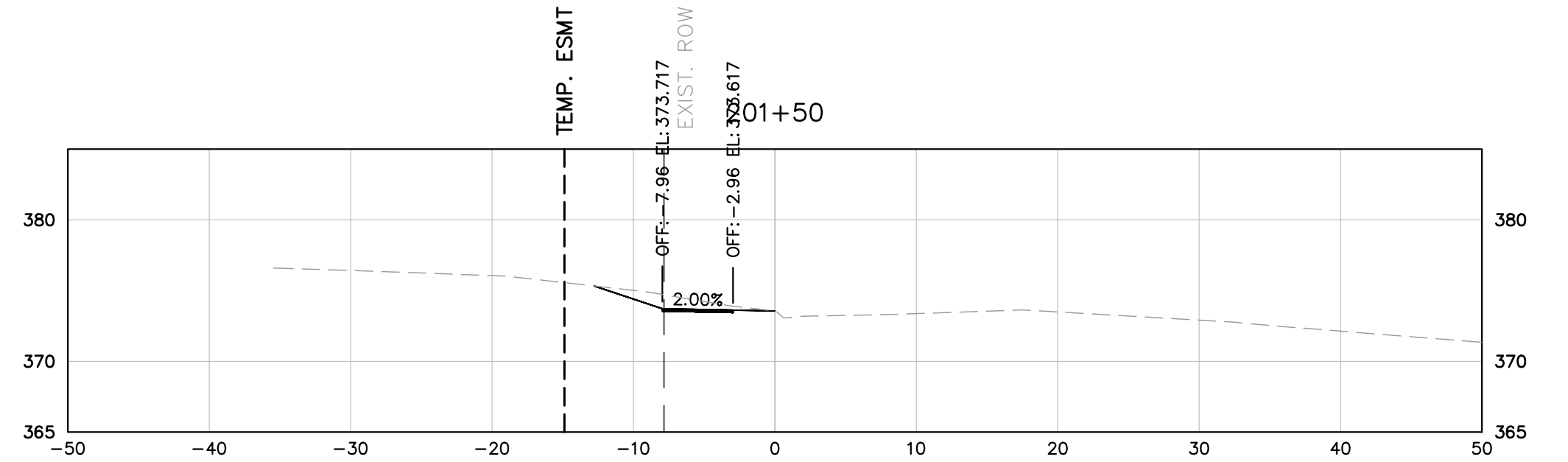
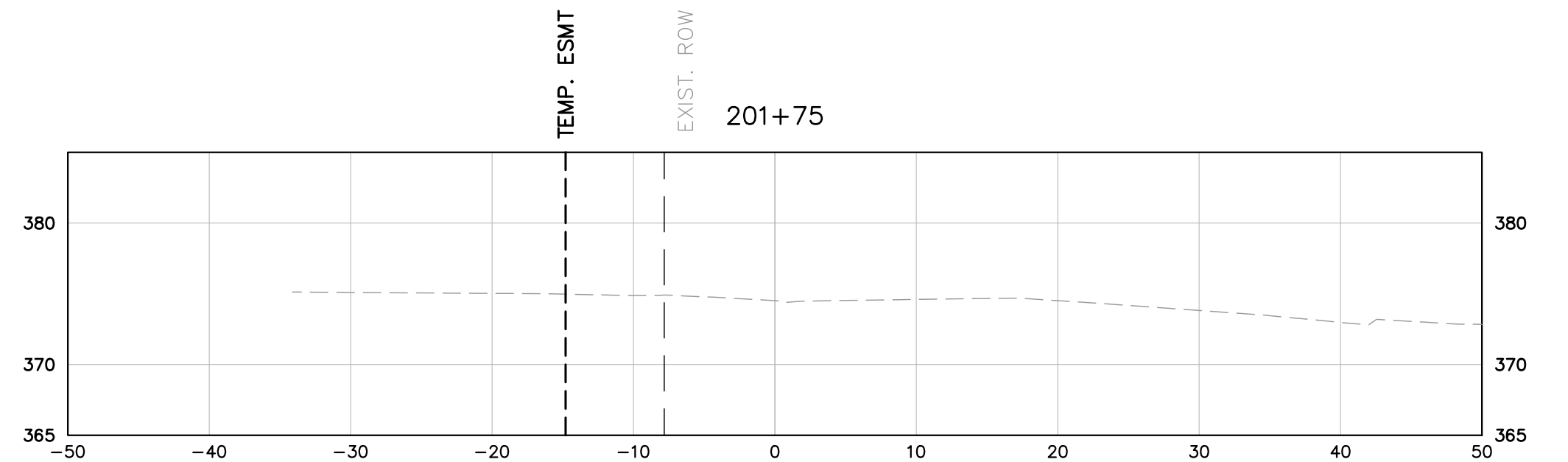
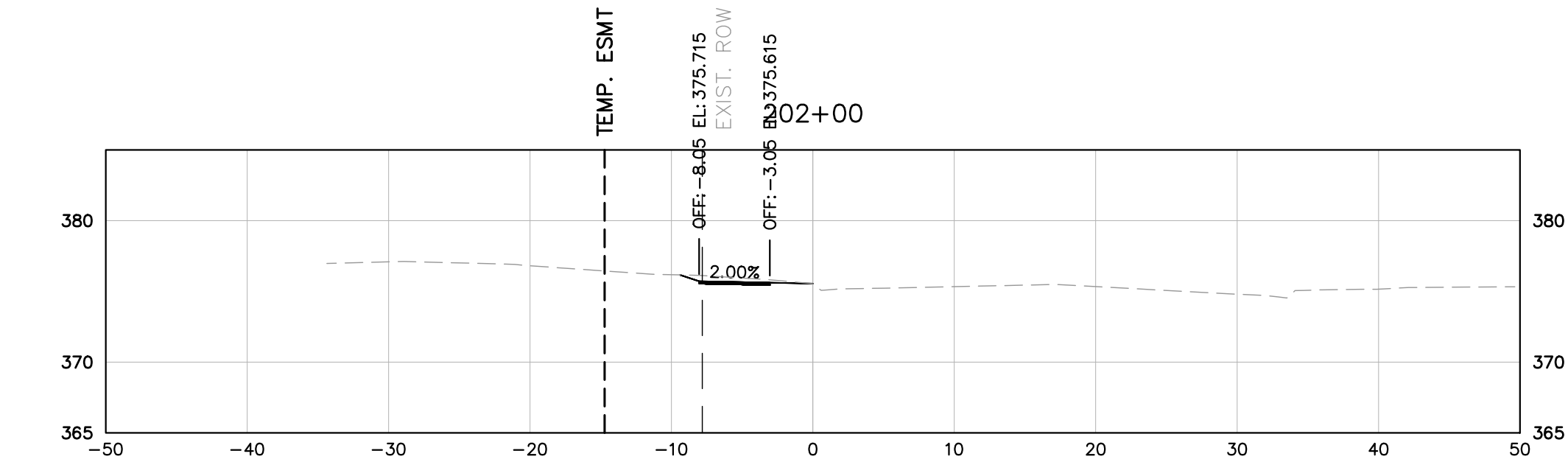
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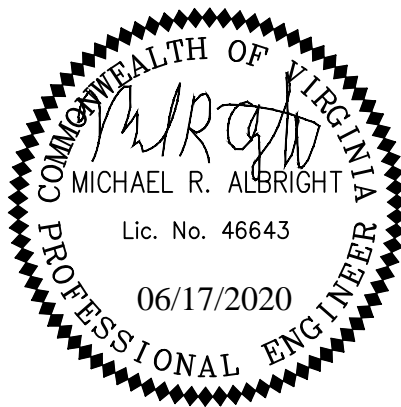
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CROSS SECTIONS - SIDEWALK

UPC # 113121

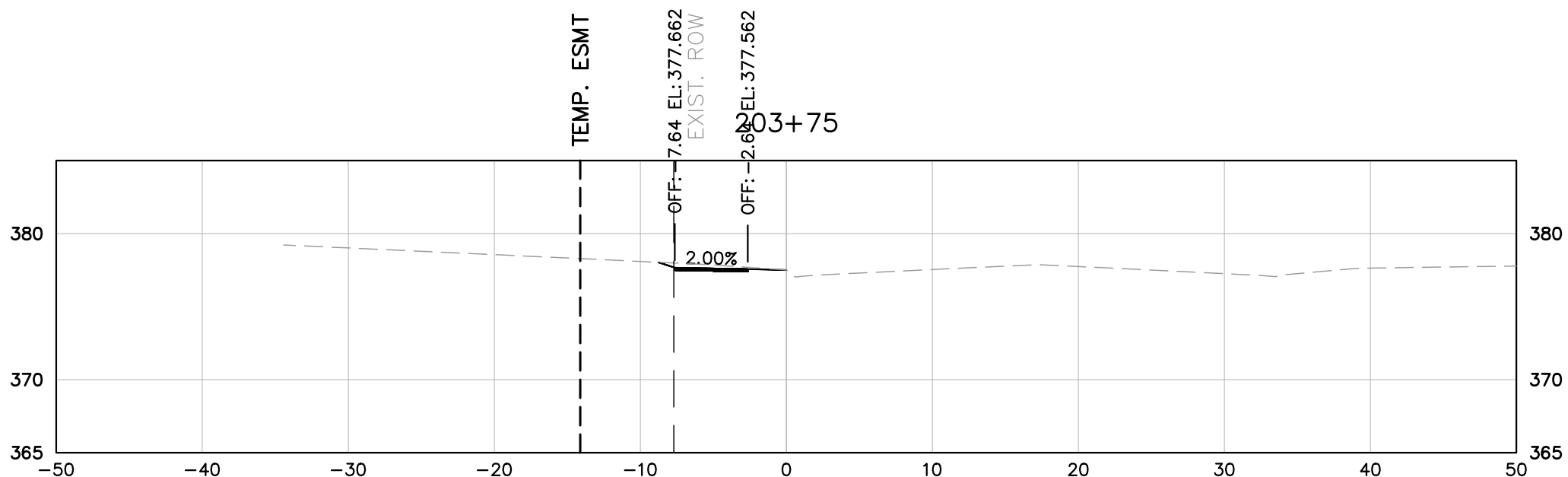
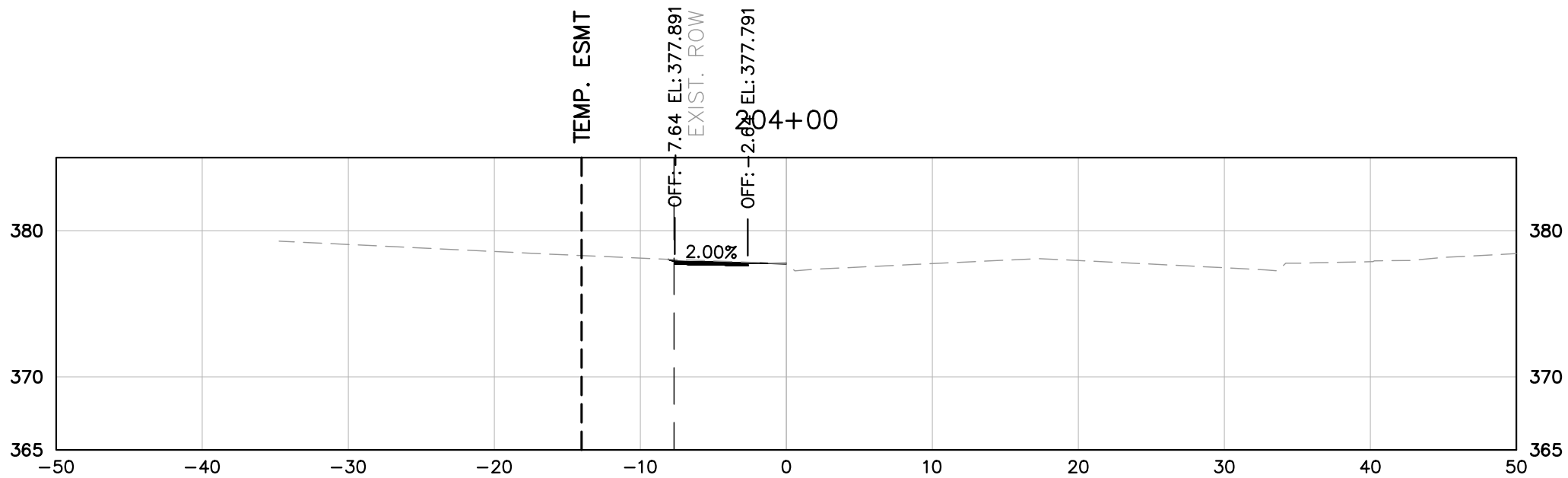
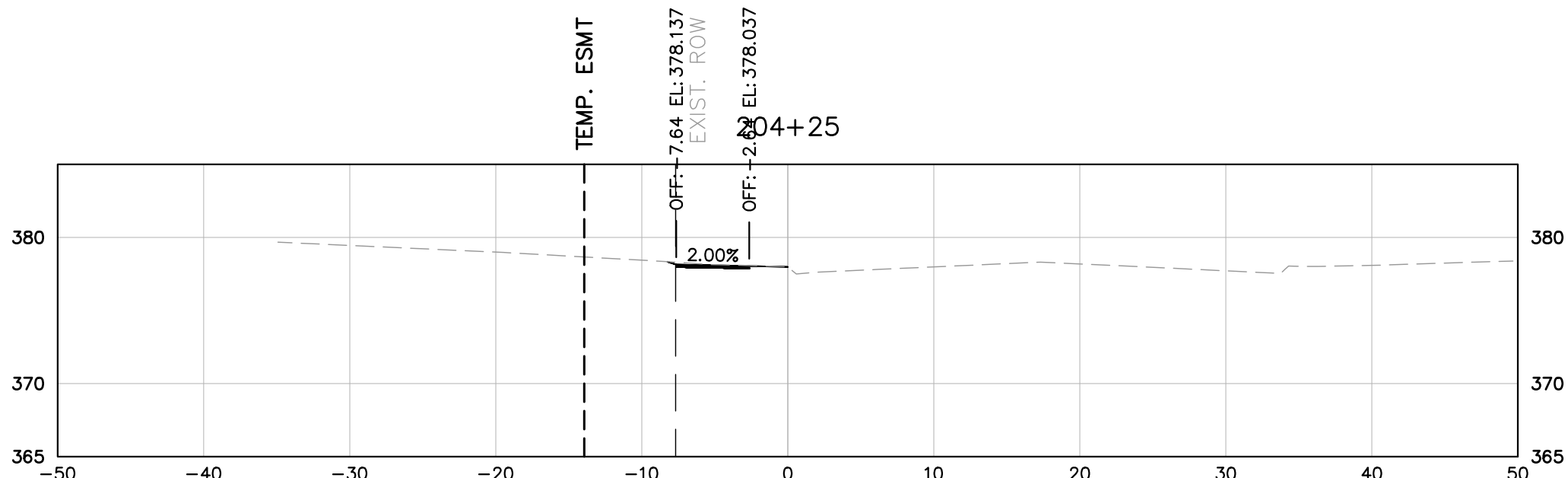
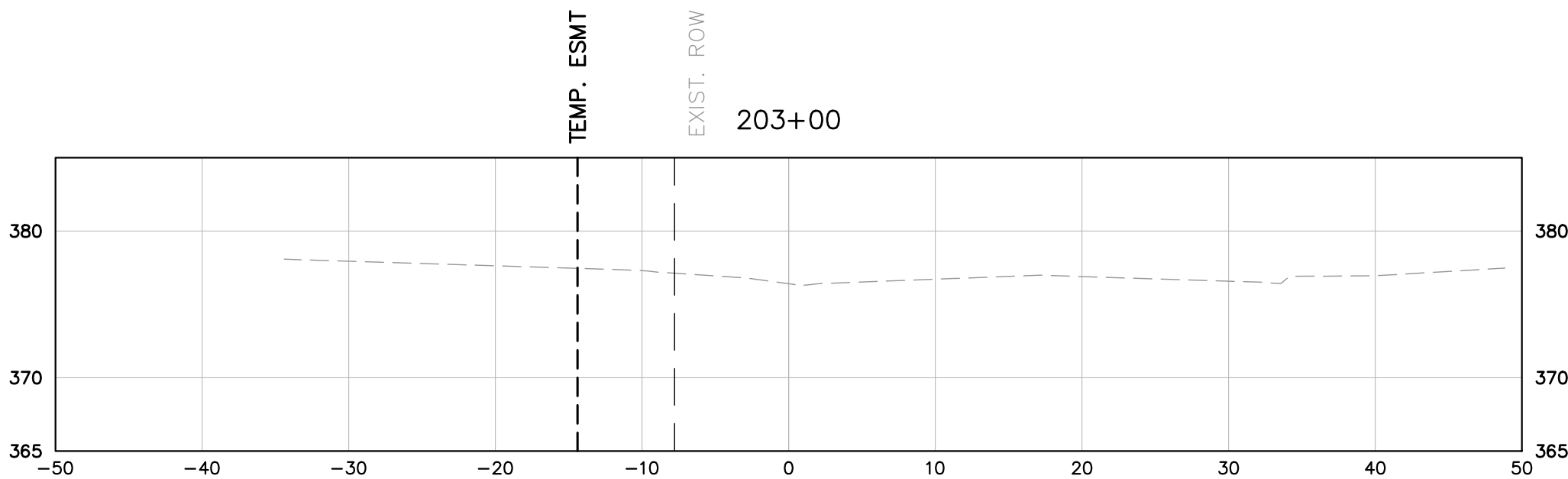
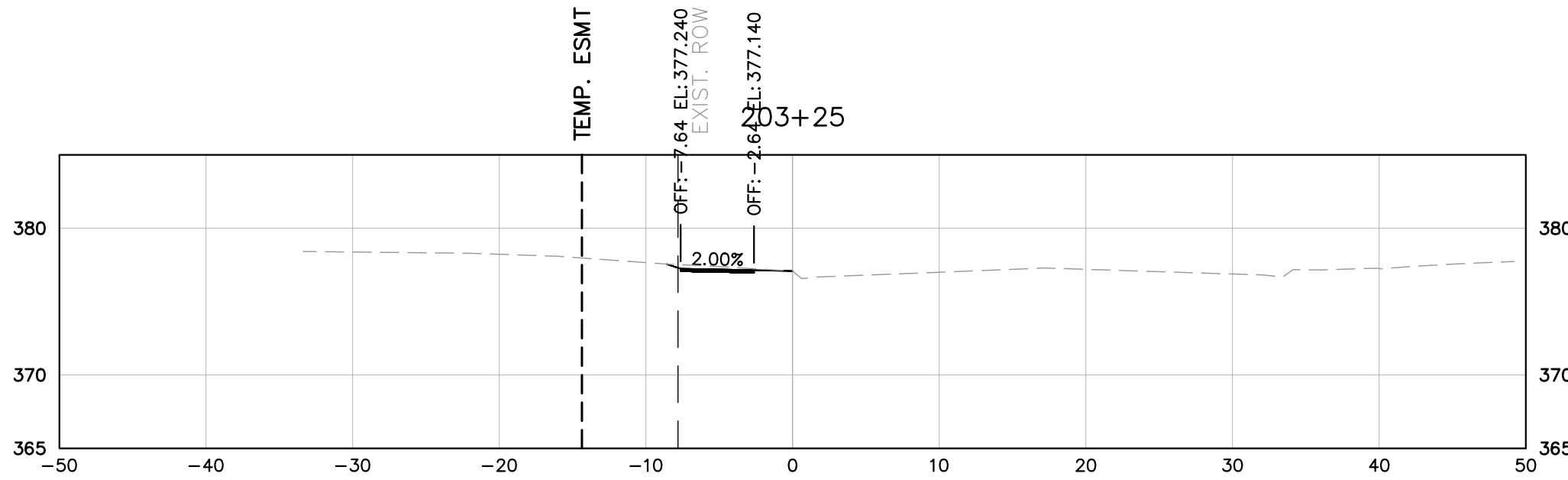
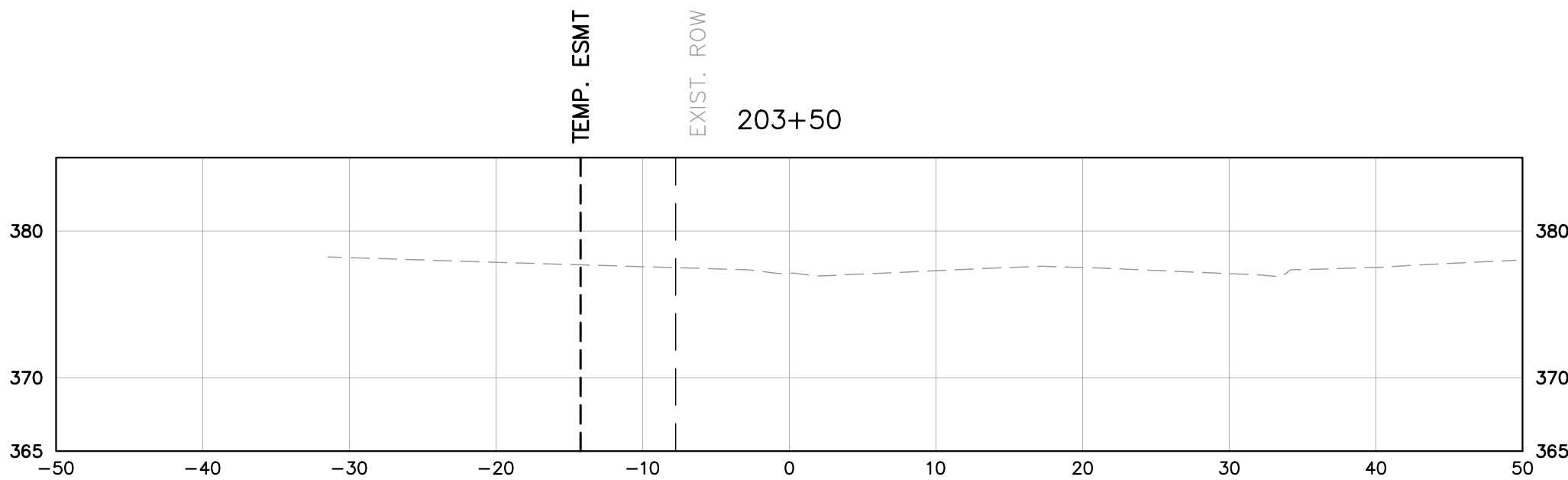
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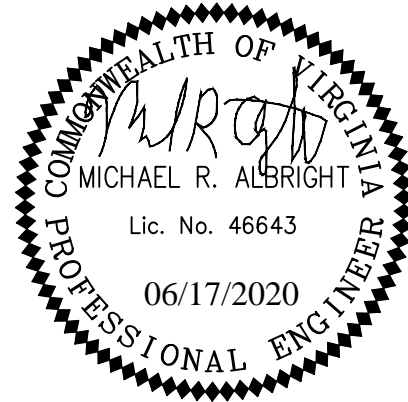
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UNIVERSITY DRIVE TRAFFIC CALMING  
KENMORE DRIVE TO STRATFORD AVENUE  
CROSS SECTIONS - SIDEWALK

UPC # 113121

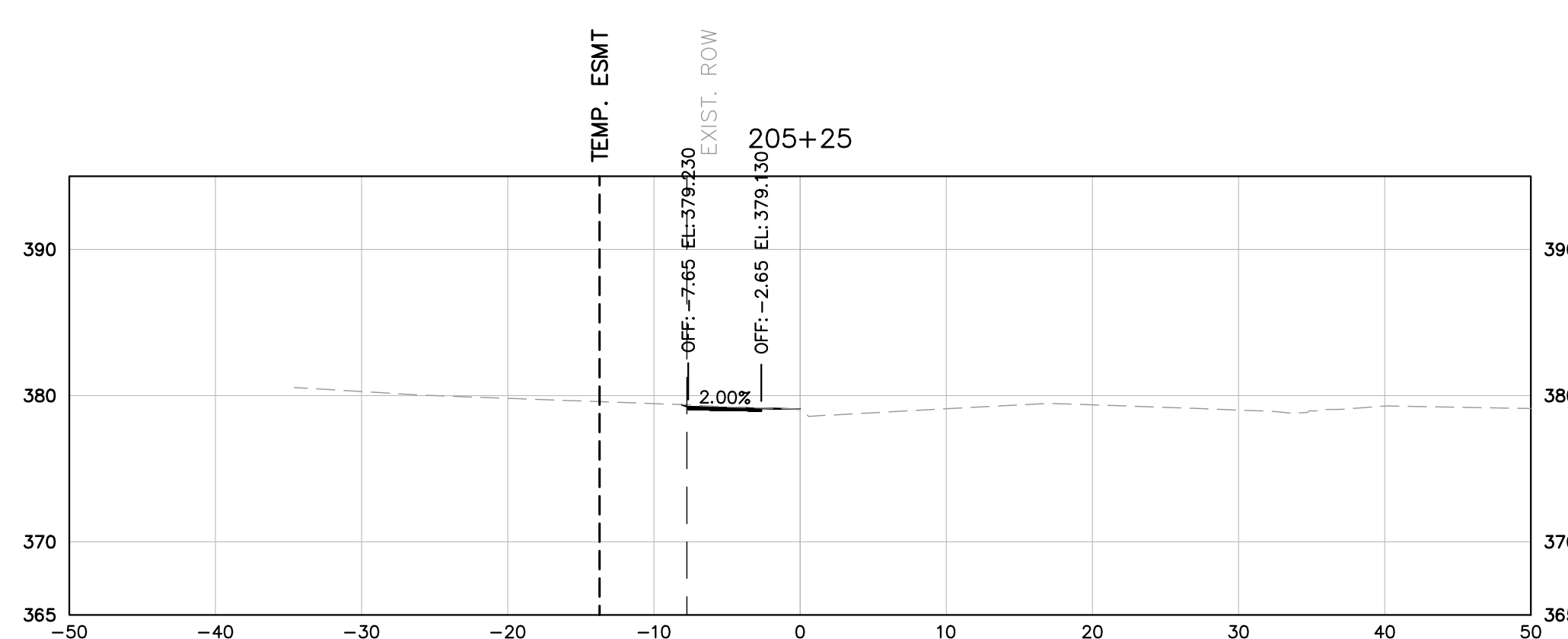
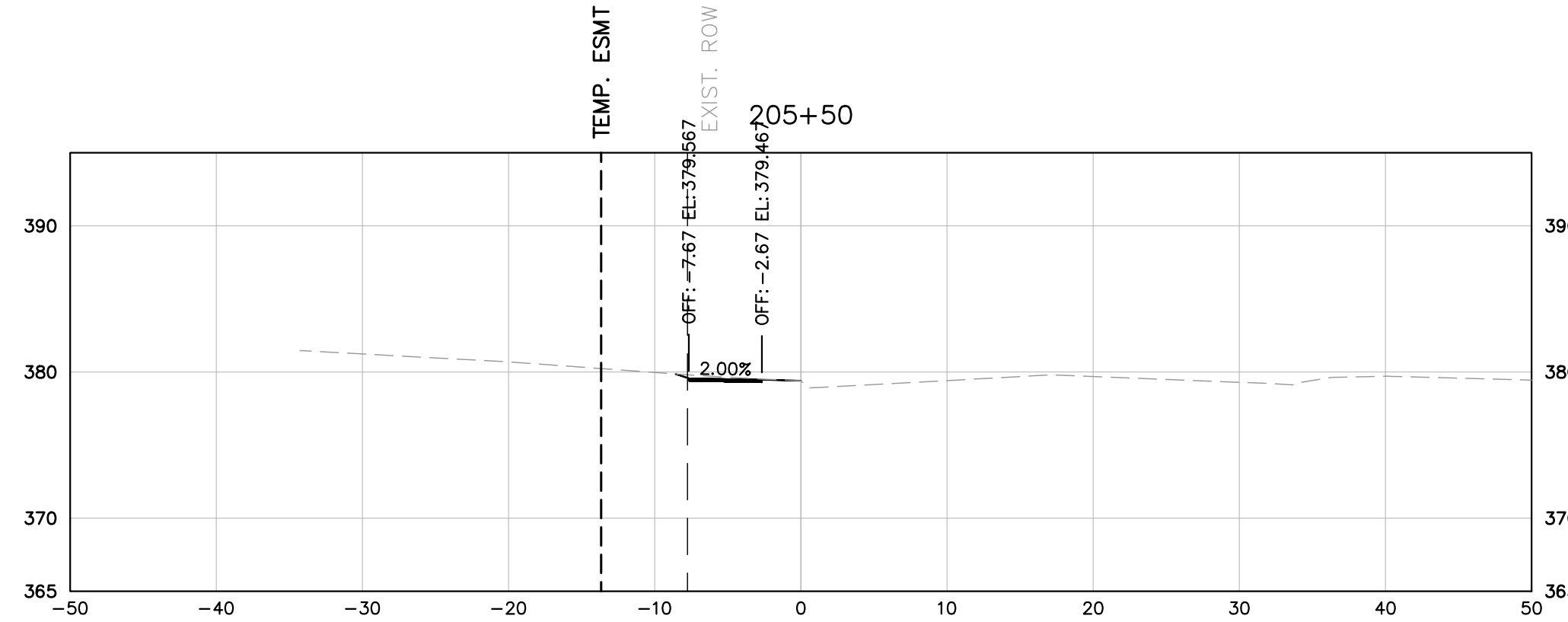
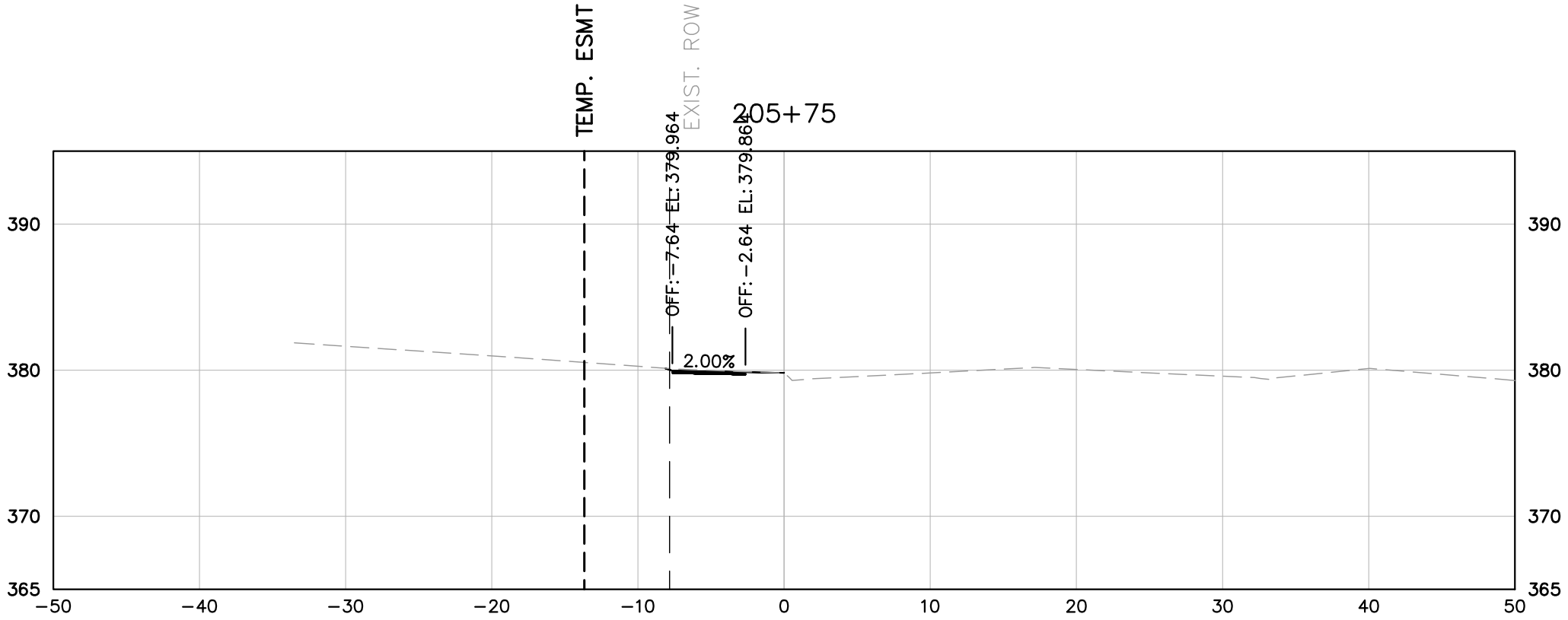
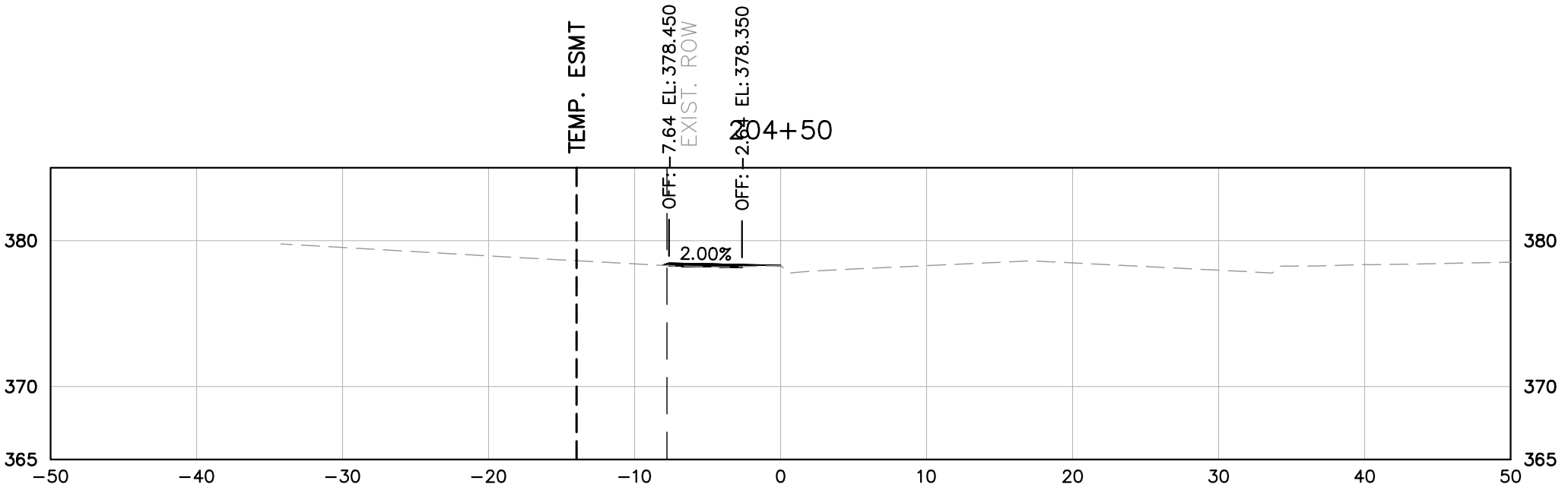
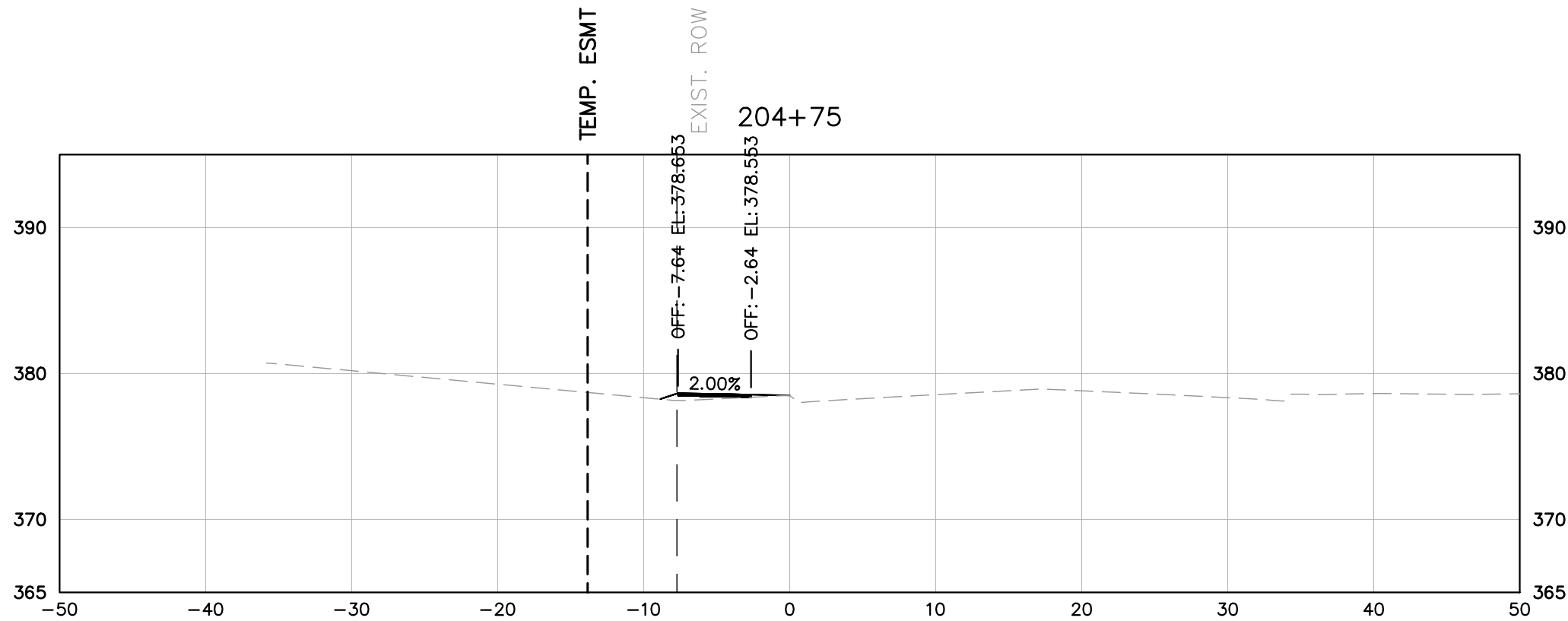
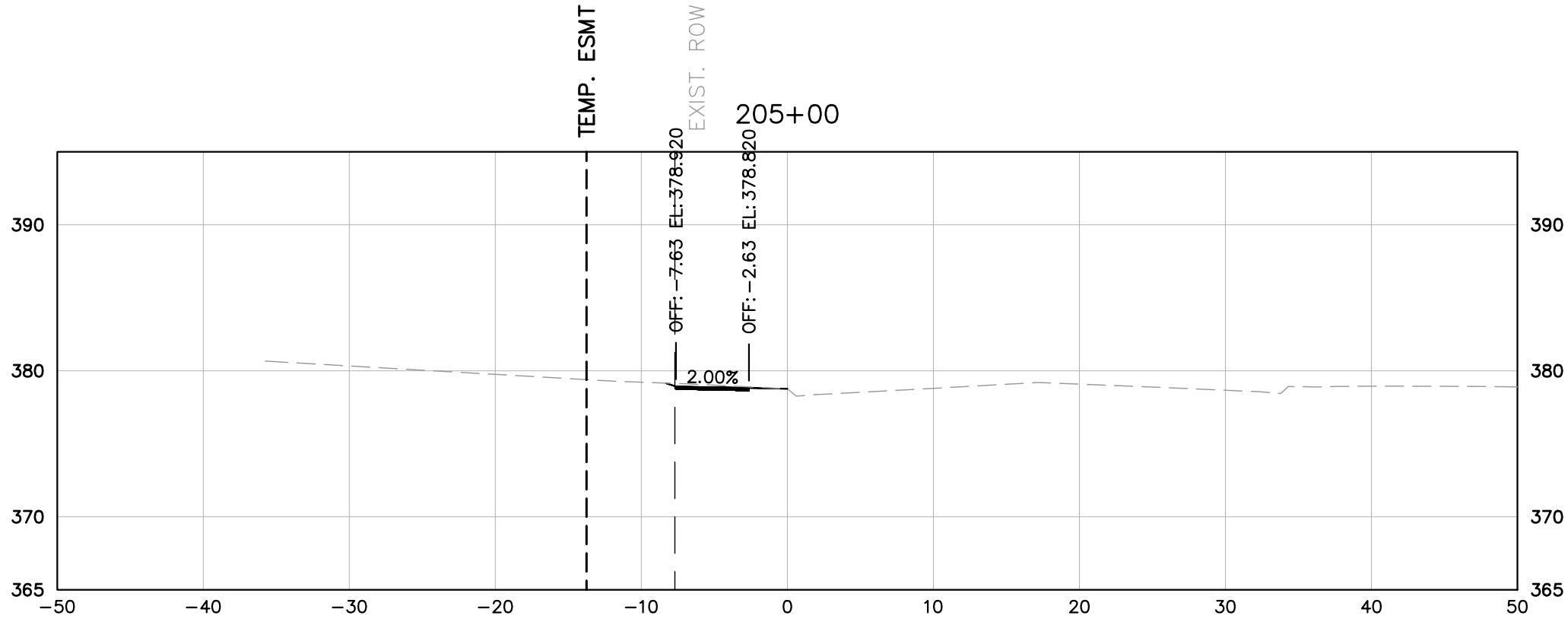
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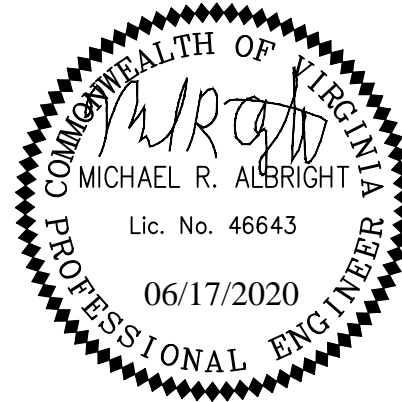
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KENMORE DRIVE TO STRATFORD AVENUE  
CROSS SECTIONS - SIDEWALK

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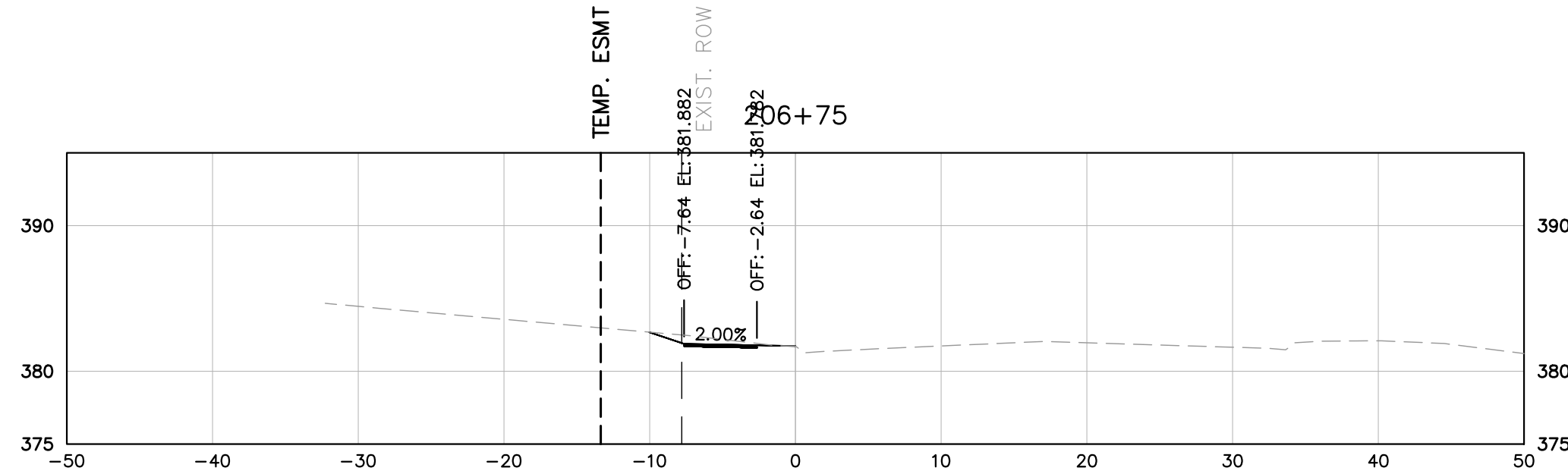
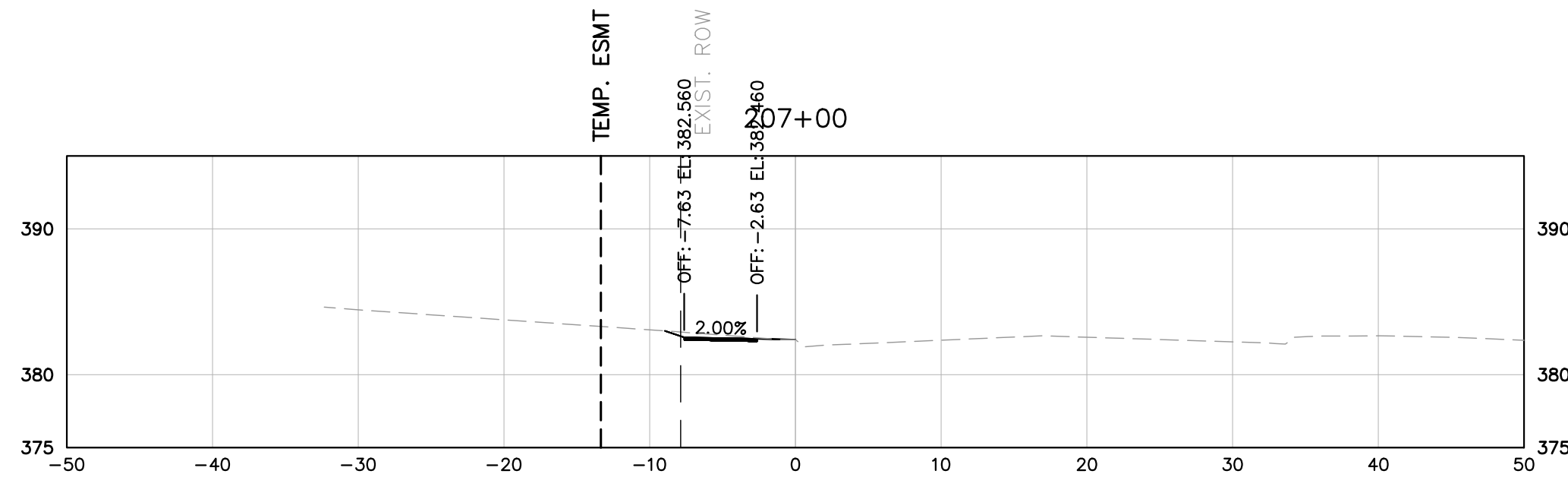
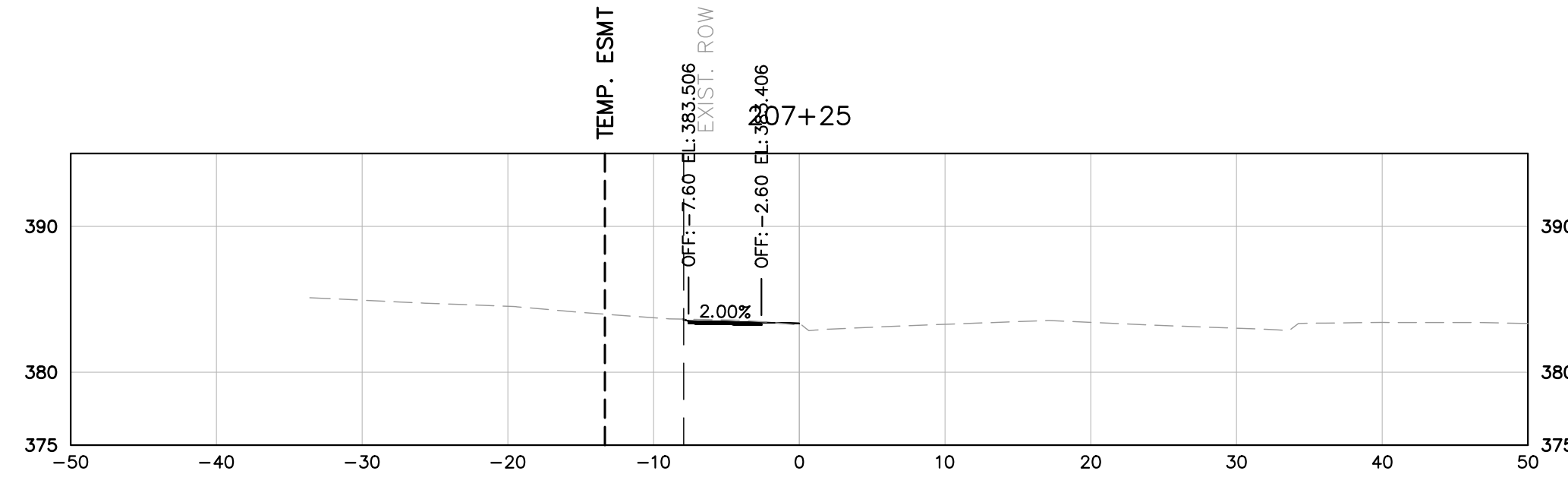
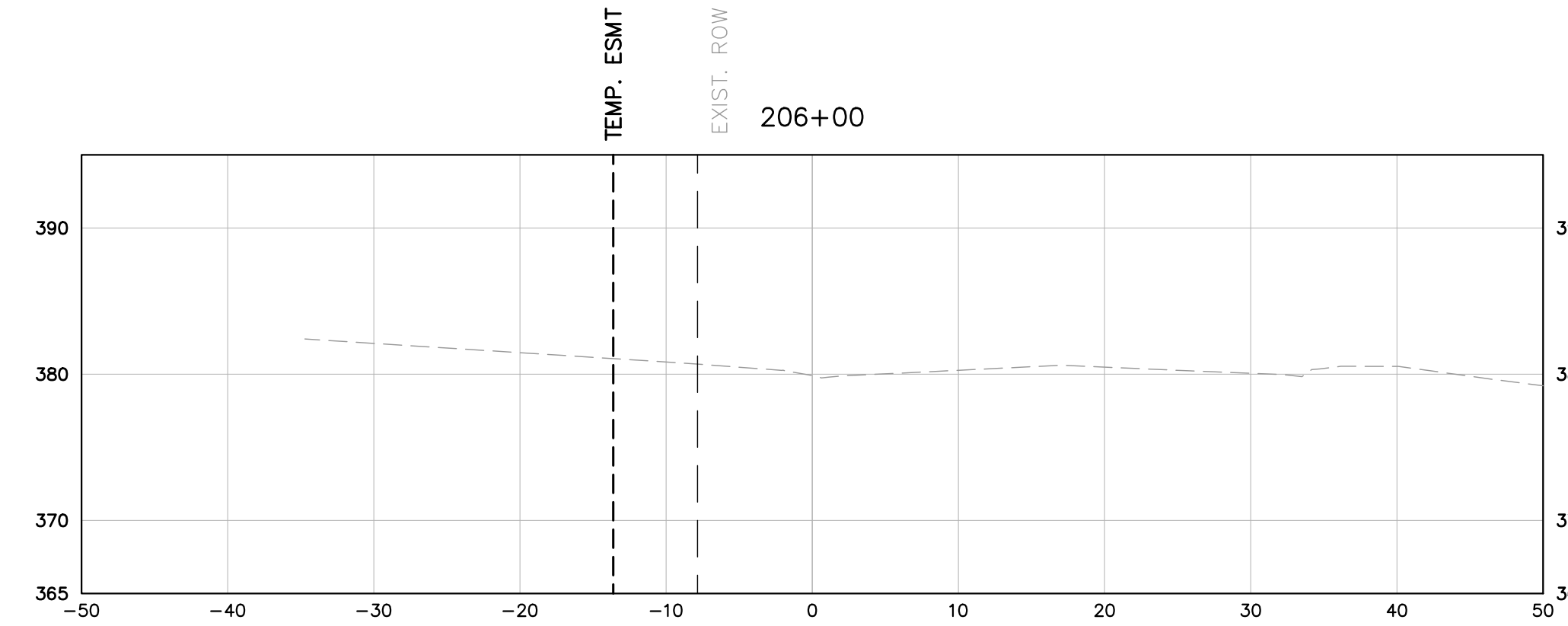
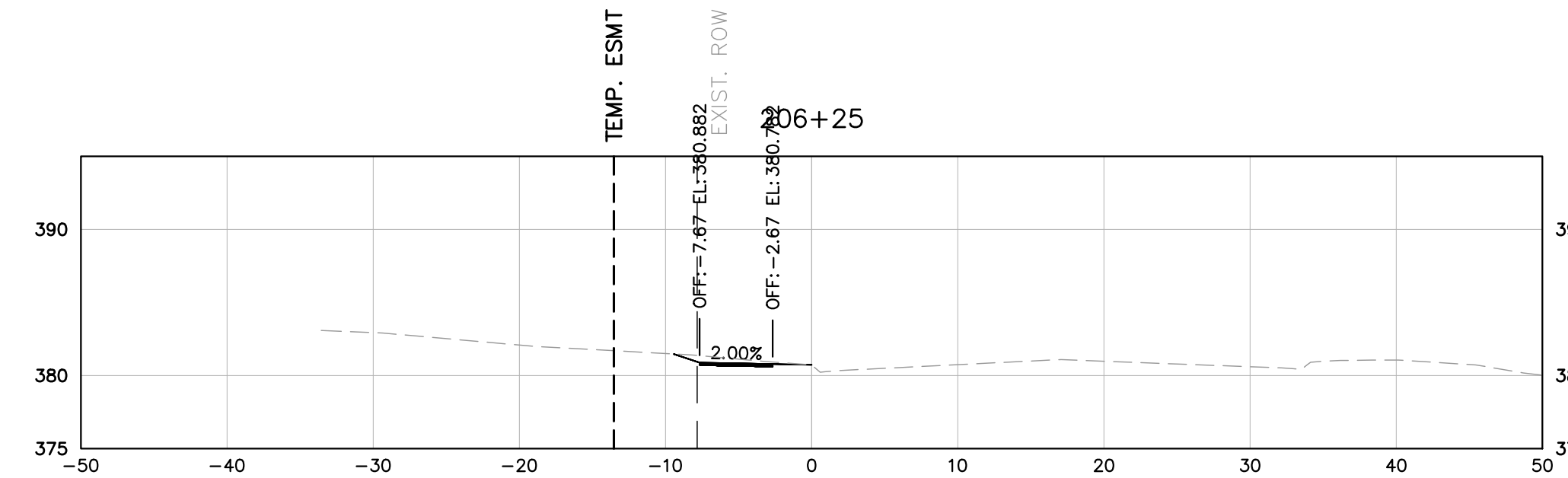
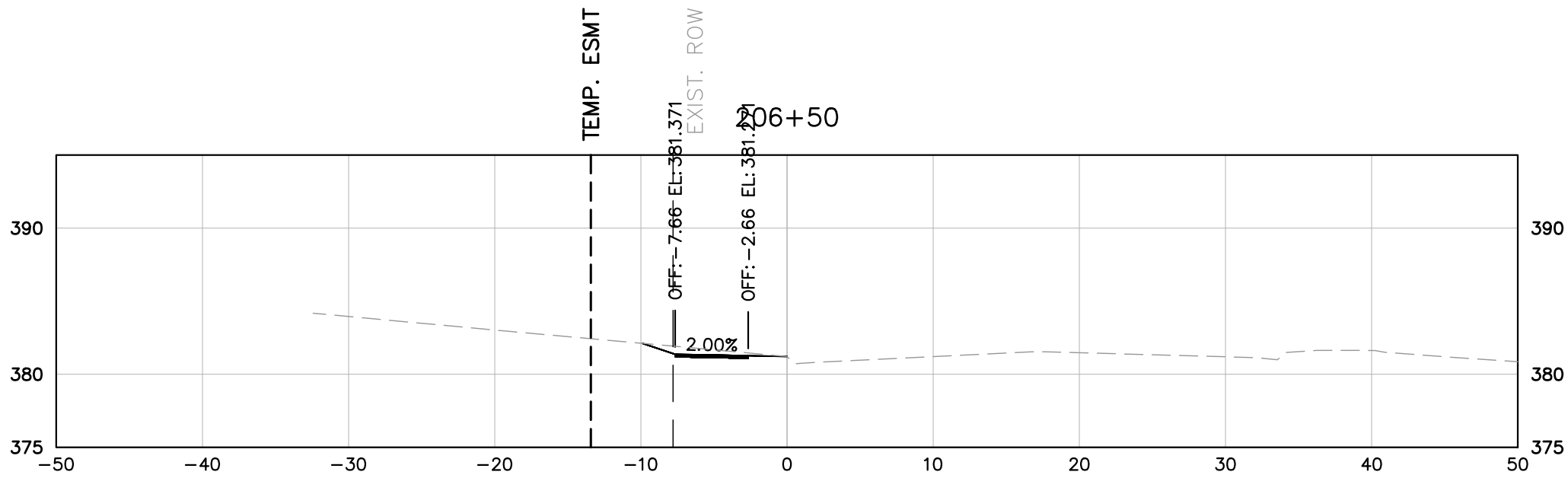
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KENMORE DRIVE TO STRATFORD AVENUE  
CROSS SECTIONS - SIDEWALK

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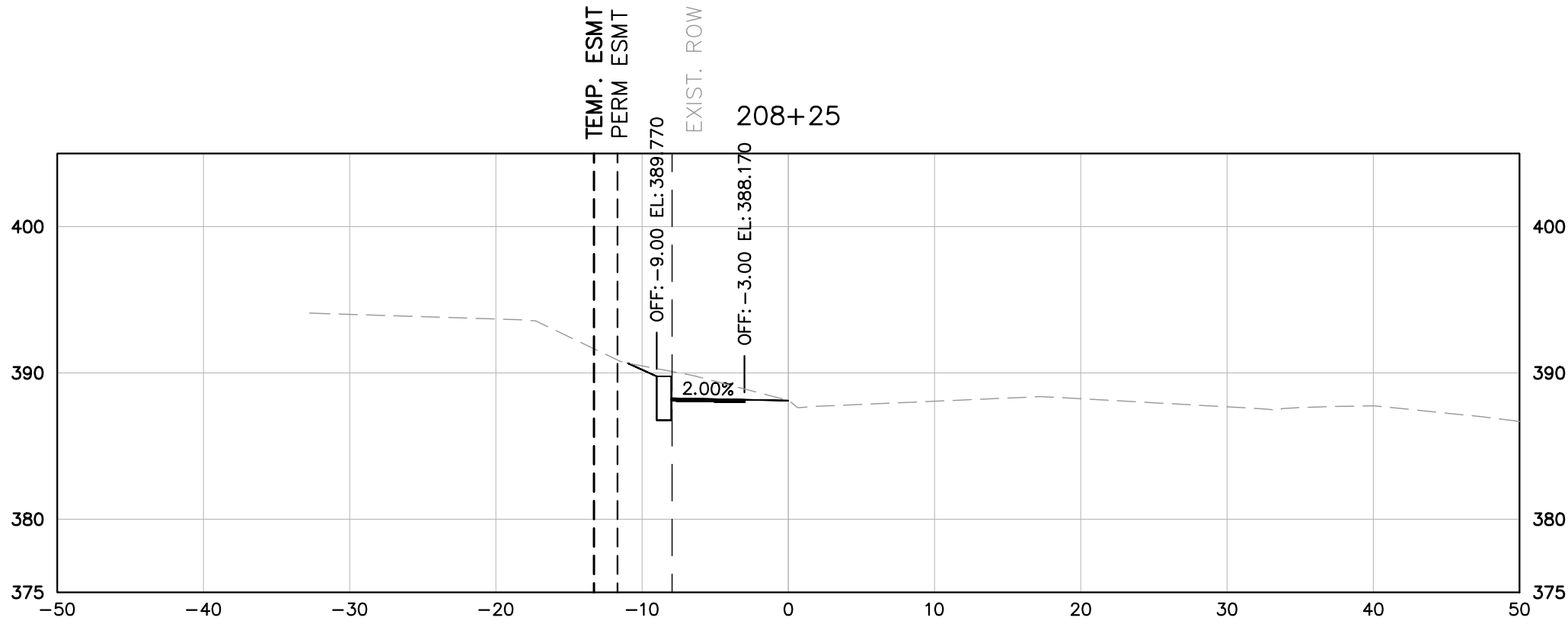
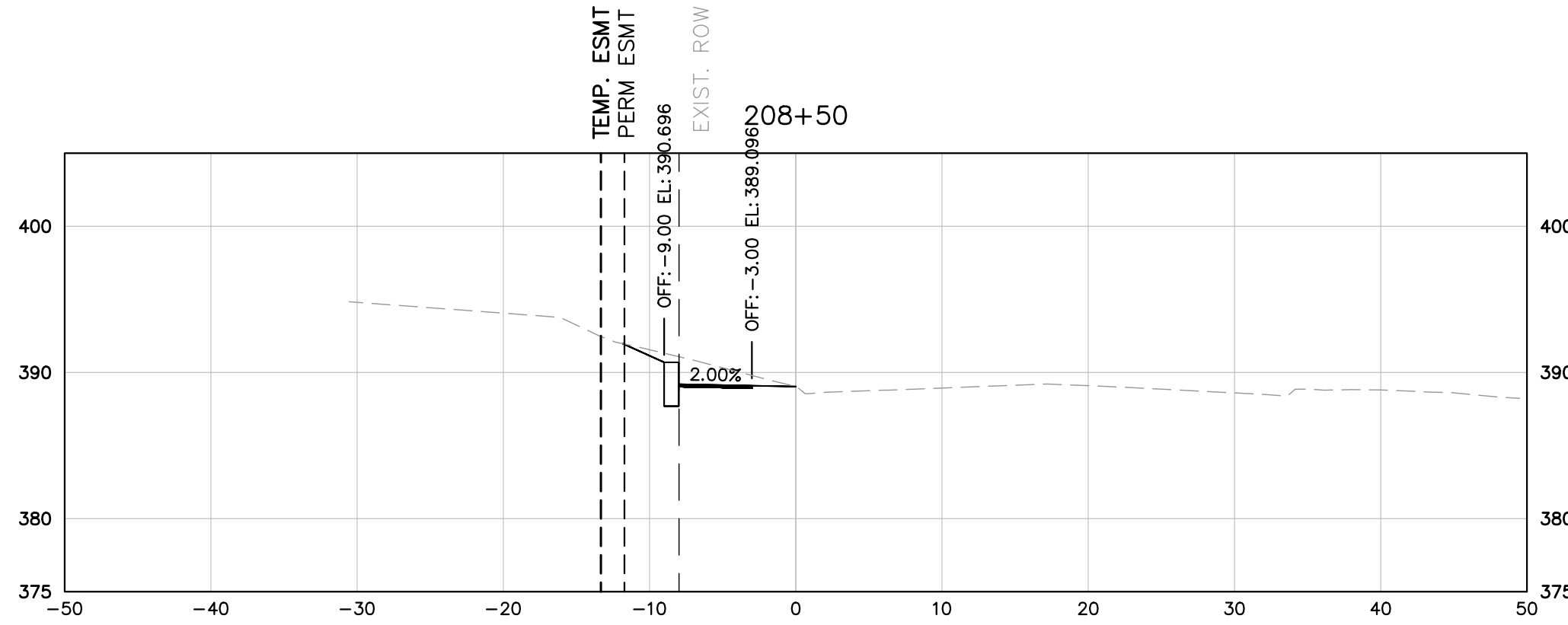
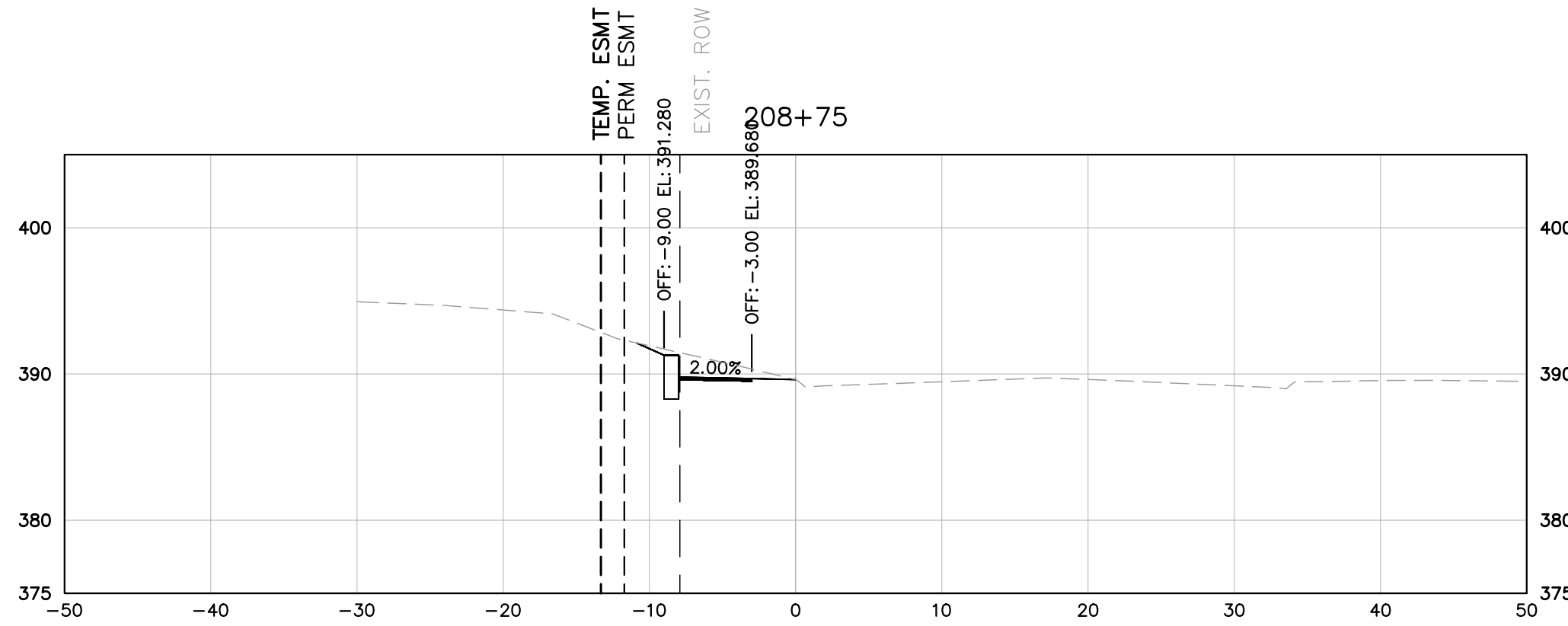
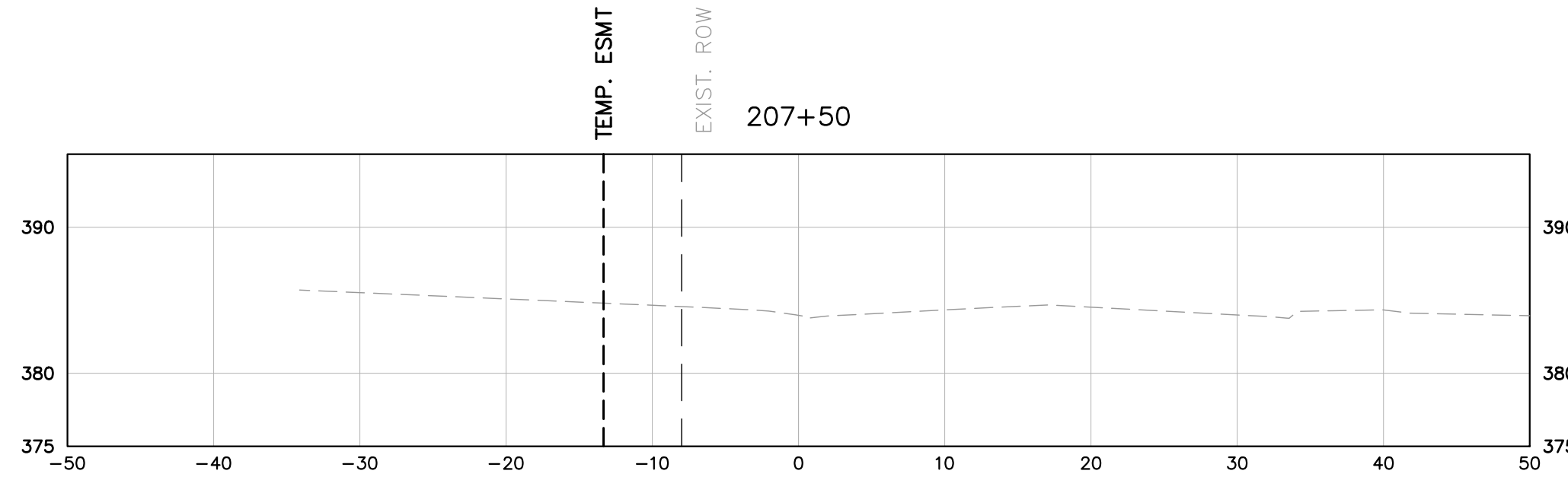
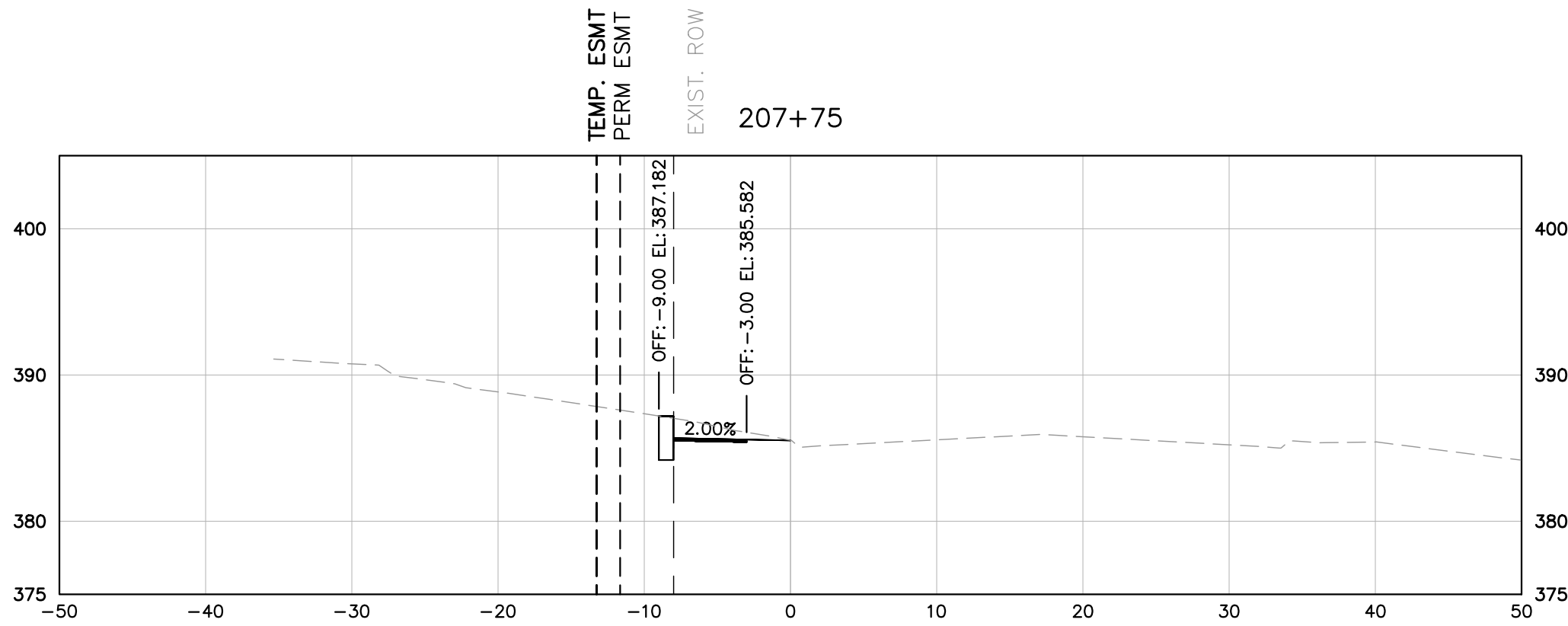
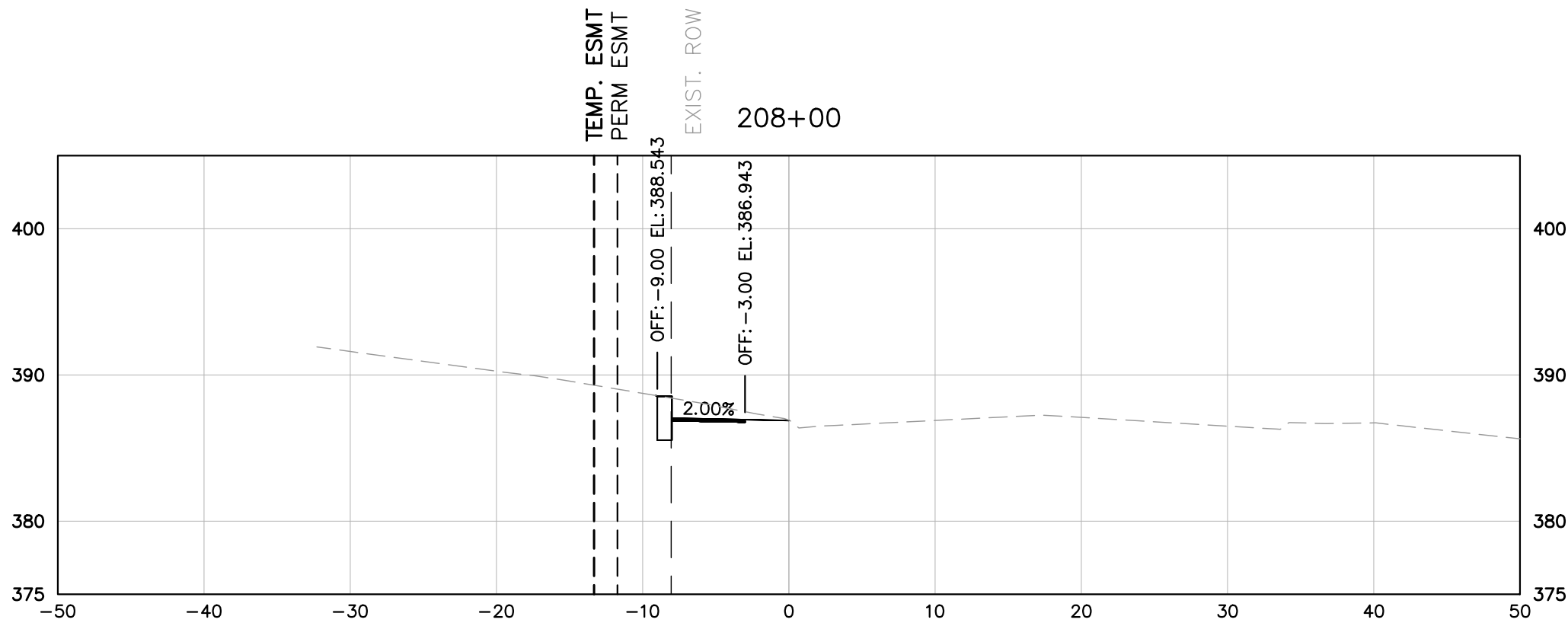
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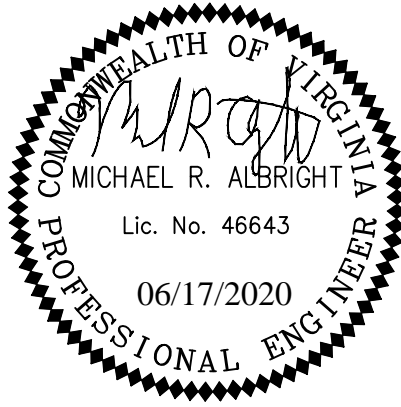
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CROSS SECTIONS - SIDEWALK

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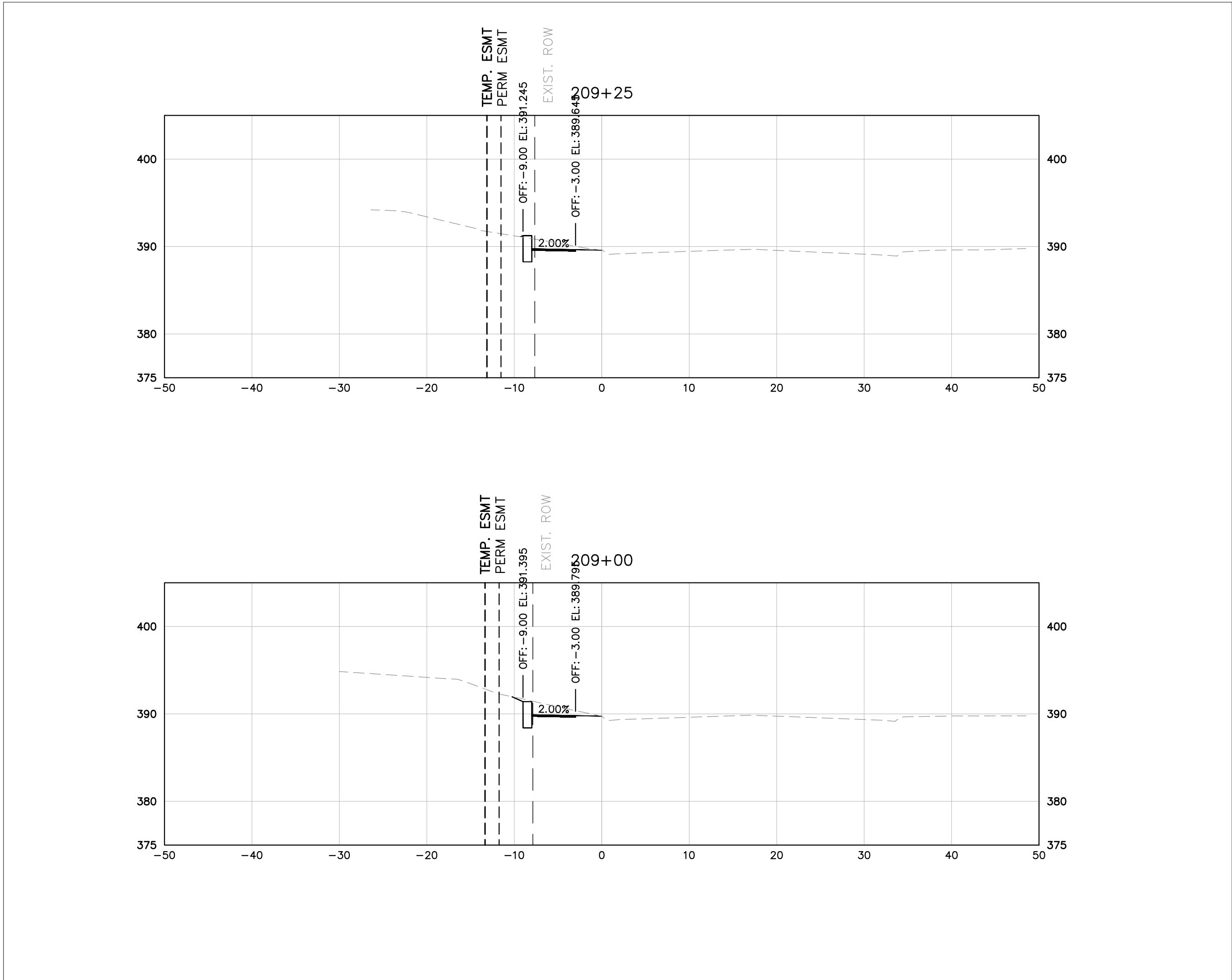
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